

**AGENDA
PLANNING COMMISSION MEETING
CITY OF SHOREVIEW**

DATE: MAY 22, 2012
TIME: 7:00 PM
PLACE: SHOREVIEW CITY HALL
LOCATION: 4600 NORTH VICTORIA

1. CALL TO ORDER

ROLL CALL

Approval of agenda

2. APPROVAL OF MINUTES

April 24, 2012

Brief Description of Meeting Process – Chair Steve Solomonson

3. REPORT ON CITY COUNCIL ACTIONS:

Meeting Date: May 7th and May 21st, 2012

4. NEW BUSINESS

A. COMPREHENSIVE SIGN PLAN

FILE NO: 2448-12-11

APPLICANT: Target - Signcrafters

LOCATION: 3800 Lexington Avenue

B. SITE AND BUILDING PLAN REVIEW

FILE NO: 2447-12-10

APPLICANT: Church of St Odilia

LOCATION: 3495 Victoria Street North

C. PUBLIC HEARING - PLANNED UNIT DEVELOPMENT – DEVELOPMENT STAGE

FILE NO: 2449-12-12

APPLICANT: TCF Bank – Dave Kroona

LOCATION: 3836 Lexington Avenue

5. MISCELLANEOUS

*A. City Council Meeting Assignments for June 4th and June 18th
Commission Member Wenner and McCool*

B. Planning Commission workshop before the regular meeting.

SHOREVIEW PLANNING COMMISSION MEETING April 24, 2012

CALL TO ORDER

Chair Solomonson called the meeting of the April 24, 2012 Shoreview Planning Commission meeting to order at 7:00 p.m.

ROLL CALL

The following members were present: Chair Solomonson; Commissioners Ferrington, McCool, Proud, Schumer, Thompson and Wenner.

APPROVAL OF AGENDA

MOTION: by Commissioner Schumer, seconded by Commissioner Ferrington to approve the April 24, 2012 agenda as submitted.

VOTE: **Ayes - 7** **Nays - 0**

APPROVAL OF MINUTES

Commissioner Schumer noted two corrections to the March 27, 2012 meeting minutes: 1) date on first page should be March 27, 2012; and 2) Commissioner Wenner's name inserted on first page where Chair Solomonson noted his arrival.

MOTION: by Commissioner Schumer, seconded Commissioner Wenner to approve the March 27, 2012 Planning Commission minutes as corrected.

VOTE: **Ayes - 7** **Nays - 0**

REPORT ON COUNCIL ACTIONS

City Planner Kathleen Nordine reported that the City Council approved the Conditional Use Permit for David Nelson and Claire Imsland at its April 2, 2012 meeting, as recommended by the Planning Commission.

OLD BUSINESS

CONDITIONAL USE PERMIT

FILE NO: 2445-12-8
APPLICANT: Automotive Ventures Group, Inc. / Karen Properties, LL
LOCATION: 3854 Lexington Ave North

Presentation by City Planner Kathleen Nordine

The proposal is to amend the company's permit to allow limited automotive sales on the property. The reason is to be able to address vehicle forfeiture by customers who, for economic reasons, cannot pay for car repairs. The station would purchase the vehicle from the owner and resell it. The maximum number of vehicles for sale would be five. Vehicles for sale would be located in an existing parking lot adjacent to Lexington Avenue and I-694.

The Planning Commission tabled this application at its last meeting and requested further information related to signage, scope of vehicles to be offered for sale, state licensing requirements, current sales data and the proposed parking location for these vehicles.

The applicant has responded that signage would be removable window signage with no painting, pennants, or banners. Staff would require that signs be limited in size to a maximum of 11" x 17". Although state licensing requires a minimum of five vehicles for sale, the applicant anticipates not more than three vehicles would be for sale at any one time based on past history. The vehicles that would be offered for sale would be ones that were brought in for repairs that the owner could not pay for. No commercial vehicles would be sold.

The applicant would apply for a Used Vehicle Dealers License. The applicant has the required liability insurance but would also be required to submit a \$50,000 bond. Within the past year, four vehicles have been offered for sale. In the past month there have been two. Vehicles on display for sale are typically on-site for from one to three months.

The City's Development Code allows automotive sales in the C-2, General Commercial District, the zoning designation of the subject property. A Conditional Use Permit may be granted if the use is permitted in the zoning district and standards and criteria of the Code are met. There is nothing specific in the City's Code that establishes criteria for automotive dealerships.

The proposal is in compliance with the Comprehensive Plan and compatible with the neighborhood. No additional improvements are needed for this proposed use. Parking capacity on the property can accommodate the proposed additional use. Staff is recommending that the Planning Commission recommend approval to the City Council with the conditions listed in the staff report that include prohibition of use of the reader board sign for automotive sales and reimbursement to the City for any enforcement issues.

Commissioner Ferrington asked what provision is made for the Conditional Use Permit to be revoked if there are violations. Ms. Nordine explained that the permit can be revoked if there are violations to the conditions placed upon the permit. A violation not related to automotive sales would not be a basis to revoke this Conditional Use Permit. She noted that there are several Conditional Use Permits on this site that will be consolidated to the extent possible when this is presented to the City Council.

Commissioner Proud agreed with the consolidation and stated that because of noncompliance with the reader board and other signage, he would like to see leverage for City enforcement within the conditions of the Conditional Use Permit. Condition No. 11 does not cover all City enforcement costs. City Attorney Filla stated that if the applicant is in violation of City Code, the City can pursue enforcement. Costs can be requested from the court.

Commissioner Proud responded that although he has not represented the City, in other instances he has not been successful in obtaining costs through the court, unless it was in a written contract. That is why he does not believe the City had the necessary leverage for enforcement. City Attorney Filla stated that the Conditional Use Permit is for the use, not the entire site. Commissioner Proud requested that the enforcement provision be written to cover the entire site.

Commissioner McCool suggested a condition that would include employees not parking in the spaces for auto sales. Ms. Nordine stated that staff does not believe there is a parking issue on the site, which is why such a condition was not included.

Commissioner Ferrington stated that her concern is that it will take significant enforcement to insure this site remains in compliance. She would like to see language used in future applications that builds in a way for the City to recoup enforcement costs.

Commissioner Wenner referred to condition No. 8, "Vehicles offered for sale on this property shall include only those acquired by the service station operating on this property. These vehicles shall include only those forfeited or acquired from service station customers. Vehicles from other service stations, including those owned by the same operator, shall not be offered for sale on this property." He suggested a change to the first sentence that would read, "Vehicles offered for sale on this property shall be limited to those vehicles acquired...". This would prohibit vehicles brought in from other locations to be sold from this site.

Commissioner Schumer noted that the third sentence in condition No. 8 addresses that concern.

Commissioner Solomonson noted that the Sinclair station across the street is owned by the applicant and vehicles can be sold from that site.

Commissioner Proud asked if there are restrictions on the signage for selling vehicles in regard to color, lighting, whether the reader board can be used. Ms. Nordine stated that no lighting is allowed, nor use of the reader board. The signage would be paper. Color is not addressed.

Mr. Mark Mclean, 3850 Lexington Avenue N., stated that the purpose of this application is not to become a used car lot. The purpose is to be able to have a few vehicles for sale as the need

arises. It can be done subtly and as a service to the City. It is a growing customer demand from people who cannot afford their car repairs. Also, people ask if he knows of a good used car. This use needs to be part of his business plan. In the year he has been on this property, no vehicles have been sold from the Sinclair site. The state requirement is to have five stalls for cars for sale. He does not anticipate more than two or three at a time. He is not required to have five vehicles in those required five stalls. Having a state license makes it easier for title work, administration and the fact that detailed must be kept to comply with the license. The license also assures customers. A license would also require that a sale would be subject to the No Lemon Law. Signs are planned, but it will not be a big part of the business. Advertising can also be done on the internet. The stalls can be designated to keep traffic flow efficient. Keeping the sales subtle will not impact the new businesses coming into the area. The restrictions proposed for the Conditional Use Permit are reasonable.

Commissioner Proud thanked **Mr. Mclean** for his forthright answers to the Commission's questions. He asked if any sales would be on consignment and suggested black and white signage with no lights or digital. **Mr. Mclean** stated that he will use professional signs. There will be no consignment sales.

Commissioner McCool asked if there is a need for the ability to sell vehicles on both the Sinclair site and the subject site. **Mr. Mclean** stated that he is willing to rescind his ability to sell cars at the Sinclair site. It would not be feasible to only sell cars at the Sinclair site because of the volume difference between the two sites.

Chair Solomonson opened the discussion to public comment. There was none.

Chair Solomonson stated that with such tight restrictions for this Conditional Use Permit, his concern is allowing this additional use. There is a lot going on commercially in this area. He is not in favor of changing the Conditional Use Permit.

Commissioner McCool noted that the Conditional Use Permit granted in 1983 requires reimbursement to the City for all enforcement costs. This condition would still be in place, and he agrees with staff that all of the Conditional Use Permits granted for the site should be consolidated with reimbursement subject to all conditions of all the Conditional Use Permits. This is a use that works. The conditions placed on the use will keep it low volume and work for the concerns expressed by the Commission. This is helping a business out that is making a reasonable request.

Commissioner Schumer agreed stating that the additional use is simply an expansion of the existing business, not something new. It is a natural progression that will be low volume. What is proposed is part of doing business, and he supports the application.

Commissioner Ferrington stated that she is not comfortable bringing used car sales to the City. However, the use is allowed regardless of personal feeling. City Attorney Filla stated that the use can be regulated but not prohibited.

Chair Solomonson asked how the use was prohibited in the first place. City Attorney Filla responded that he would have to look to see if, at an earlier time, the use was prohibited by Code.

Commissioner Proud stated his support with his suggested changes to condition Nos. 6 and 8.

Commissioner Wenner agreed and stated that the applicant has explained how his industry has changed. These situations are presented to him, and he is trying to provide a service. The Code allows this use and the City should help to solve this issue.

Commissioner Thompson stated that she would support the application. Her concern of placement of parked cars and traffic flow have been addressed.

Chair Solomonson stated that this would set a precedent for increased car sales in the zoned C-2 areas in the City. Also, there is nothing to explain why car sales were prohibited on this site. He would like to see the Code revisited in this instance.

MOTION: by Commissioner Schumer, seconded by Commissioner McCool to recommend the City Council approve the Conditional Use Permit for Automotive Ventures, Inc./Karen Properties, LLC allowing automobile sales on the property at 3854 Lexington Avenue. Said approval is subject to the following conditions with one change in condition No. 6, line 2, to read that window signage shall be non-electronic and not lighted and shall not exceed the size of 11” x 17” in area; and approval is based on the four findings listed.

- 1) This amendment permits automobile sales on the property provided the maximum number of vehicles for sale at any one time does not exceed 5.
- 2) Condition No. 4 in Conditional Use Permit 971-83-22 which states “no vehicles may be offered for sale on the property” is rescinded.
- 3) Condition No. 4 in Conditional Use Permit 99-18 is hereby modified as follows. The parking area shall be used for employee parking, the parking of customer vehicles currently being serviced and vehicle sales as conditioned with this permit. Exterior storage is not permitted on this property. All vehicles shall be licensed and operable. No vehicles, with the exception of those for sale, shall be stored on the site for more than 30 consecutive days.
- 4) Automobile vehicles for sale shall be limited to passenger vehicles only. The sale of commercial vehicles is prohibited.
- 5) Automobile vehicles for sale shall be displayed on the property in the parking areas as identified in the submitted site plan.
- 6) Additional signage advertising car sales is not permitted on the property, with the exception of window signage displayed in the vehicle being offered for sale. Said window signage shall not exceed 11” x 17” in area. The existing message center sign shall not be used to advertise vehicles for sale.
- 7) Third party sales are prohibited.
- 8) Vehicles offered for sale on this property shall include only those acquired by the service station operating on this property. These vehicles shall include only those forfeited or acquired from service station customers. Vehicles from other service stations, including those owned by the same operator, shall not be offered for sale on this property.

- 9) Records shall be made available at the City's request documenting the acquisition and sale of vehicles. Said records shall include, but may not be limited to: reason for forfeiture, acquisition method, date of acquisition from the seller, and the date sold by the dealership,
- 10) The Permit holder must have a valid Minnesota motor vehicle license to sell cars on this property.
- 11) Permit holder shall reimburse the City for all costs incurred, including legal fees, in order to enforce the terms of this conditional use permit.

This approval is based on the following findings:

- 1) The use is in harmony with the general purposes and intent of the Development Ordinance.
- 2) The use is in harmony with the policies of the Comprehensive Guide Plan.
- 3) Certain conditions as detailed in the Development Ordinance exist.
- 4) The structure and/or land use conform to the Land Use Chapter of the Comprehensive Guide Plan and are compatible with the existing neighborhood.

DISCUSSION:

Commissioner Proud offered an amendment to the end of condition No. 8 that would read, "These vehicles shall include only those forfeited or acquired from service station customers in satisfaction of unpaid repair charges."

Commissioner McCool noted the situation where someone came in and could not afford the repairs. The station then bought the car, made the repairs and sold it. In that instance, there would have not been a satisfaction.

Commissioner Schumer stated that he cannot support the amendment because of the example described by Commissioner McCool.

Commissioner Proud stated that he would not want to support instances with vehicles brought in to be sold before the station makes the repair. That is operation similar to a used car lot. Commissioner Wenner seconded the amendment.

Commissioner McCool asked **Mr. Mclean's** reaction to the amendment. **Mr. Mclean** stated that unpaid repair does not cover the charge for diagnostics on the car. The amendment complicates the matter. Commissioner McCool stated that he would vote against the amendment because the application is for cars that are brought in for repairs but cannot afford the work. He also does not want to see this turned into an open sales site, but the amendment stipulating just satisfaction of charges does not give the business what it needs.

VOTE ON FIRST AMENDMENT:

Ayes - 2 (Proud, Wenner) Nays - 5 (Ferrington, McCool, Schumer, Thompson, Solomonson)

The amendment failed.

Commissioner McCool offered an amendment to the second sentence of condition No. 8 to read, "These vehicles shall include only those forfeited or acquired from customers whose vehicles are at the service station for repairs." Commission Ferrington seconded the amendment.

VOTE ON SECOND AMENDMENT:

Ayes - 7 Nays - 0

The amendment was adopted.

VOTE ON MOTION:

Ayes - 6 Nays - 1 (Solomonson)

NEW BUSINESS

VARIANCE

FILE: **2414-11-07**
APPLICANT: **James Gruber**
LOCATION: **3289 Emmert Street**

Presentation by City Planner Kathleen Nordine

The applicant received a variance last year to reduce the front yard setback for Parcel 1 of a subdivision for construction of a future new home. The front yard setback was reduced to 40 feet. The setbacks are based on adjacent properties, which created difficulty for the setback on the subject property. The subdivision has been recorded with Ramey County, as has the variance. The subject property is now for sale. The applicant seeks an extension of the variance for one year to April 26, 2013, subject to the conditions of the variance approval remaining in effect.

Staff is recommending the extension be approved.

MOTION: by Commissioner Schumer, seconded by Commissioner Wenner to extend the variance approved for Parcel 1 reducing the front yard setback for a future home on the property to 40 feet. The subdivision and Resolution have been recorded at Ramsey County and the property is being marketed for sale. Said extension is for a one-year period to April 26, 2013. Conditions attached to the variance approval shall remain in effect.

VOTE: Ayes - 7 Nays - 0

**PUBLIC HEARING - PRELIMINARY PLAT/ COMPREHENSIVE PLAN
AMENDMENT REZONING/ PLANNED UNIT DEVELOPMENT - DEVELOPMENT
STAGE**

FILE NO: 2446-12-9
APPLICANT: LAKEVIEW TERRACE, LLC
LOCATION: 3588 OWASSO STREET

Commissioner Proud recused himself from consideration of this matter.

Presentation by City Planner Kathleen Nordine

This application is to demolish a 13,000 square foot retail center in order to build a high density apartment complex consisting of 104 units. The project includes realigning Owasso Street with County Road E at the Victoria Street intersection. Owasso Street would be relocated to the north to align with the west leg of County Road E at Victoria. This will involve relocation of public utilities and replacement of a sidewalk. A new trail will be added along the west leg of County Road E. There will also be signal improvements and medians added. Traffic is anticipated to be somewhat higher by 143 trips per day than the Midland Plaza retail center, but the distribution pattern will be different with a residential use. The road improvements are intended to improve traffic flow in this area.

Land uses abutting this property include low density residential to the south, a church and park to the west, and railroad and business park to the north. The proposed use should not adversely impact surrounding land uses. The wetland pond on the site provides separation from the low density, single-family residential neighborhood to the south.

This redevelopment would replace an underutilized property with housing that supports the City's goals by expanding housing choices and provide a rental option not currently available in the City. This good reinvestment of developed property that the City encourages.

Four applications have been submitted:

- 1) Comprehensive Plan Amendment to change Commercial zoning to RH, High Density Residential of 8 to 20 units per acre. The proposed land use (RH) Adjacent Midland Terrace is zoned RH. This proposed density for this development is 16.6 units per acre.
- 2) The preliminary plat would be created to divide the property into two parcels--one for the apartment building and one for an adjacent parking area for the apartment building. The dedication for Owasso Street right-of-way would also be platted.
- 3) Rezoning from C1, Retail Service and R3, Multi-family Residential to a PUD, Planned Unit Development. Flexibility is requested for setback requirements from Owasso Street and Victoria Street, height of the building, and the number of parking lot stalls and setback for the parking lot.
- 4) PUD redevelopment of the property with a 104 unit apartment building.

At the Planning Commission meeting on February 28, 2012, concern was expressed about setbacks, the height of the building and water quality in the wetland area. The building placement is a result of the width of Owasso Street, which is wider than the developer thought. This has shifted the building further south closer to the wetland and reduced the footprint of the building further reducing the number of units to 104. The proposed building setbacks would be 73.49 feet from Owasso Street, 66.21 feet from Victoria Street and 18.2 feet from the wetland. The wetland setback does meet the 16.5 foot required buffer.

The permitted height is 35 feet; the developer proposes 78.5 feet. However, the building height can be exceeded when there is no impact to firefighting capabilities, and the Lake Johanna Fire Department has reviewed the plan and indicated no concerns. Further, increased height can be allowed when for every foot of increased height, there is an additional foot of increased setback. Deviations from the setback requirements are proposed

The visual impact of the building to the single-family neighborhood is mitigated by the wetland/pond separation. A reduction in height would not have a significant effect. The distance of the structure from the neighborhood reduces the impact of the actual height.

Parking stalls proposed are 167, or 1.7 stalls per unit. City code requires 2.5 stalls per unit. The number of parking stalls may be reduced when shared parking is available or proof of parking is shown. As the building is adjacent to Midland Terrace, that parking lot can accommodate any overflow parking needs.

Storm water management for the site includes an underground treatment chamber for water quality before discharge into the wetland/pond. A filtration basin will treat runoff from the southwest part of the building before it is discharged into the wetland. Roof drainage is directed into the wetland. The developer is working with a consultant to further identify ways to improve water quality in the pond area.

The Environmental Quality Committee (EQC) reviewed the application at its April 23, 2012 meeting and supports the water quality work proposed and a shoreland vegetative buffer. The EQC requested collection of roof water runoff for reuse.

Notices were sent to properties beyond 350 feet, and a development sign is posted on the property. Comments received indicate some support for the road realignment. However, there is opposition to the density of development, the height of the building and its visual impact. Residents state that this development is not compatible with the neighborhood and expressed further concerns about traffic and water quality.

Staff believes the proposal supports City goals and policies related to land use, housing and redevelopment. The change in land use will not significantly impact adjacent land uses. The visual impact of the structure is mitigated by the wetland/pond, the distance from the low density residential neighborhood, and architectural design. Realignment of the road will improve traffic flow and safety. Staff is recommending that the Planning Commission recommend approval to the City Council, subject to the conditions and findings listed in the staff report. Staff has requested the developer to work further with the City on landscaping to soften the visual impact

of the building and tree replacement along Owasso Street, as well as consider the recommendations of the EQC.

Commission Discussion

Commissioner Wenner asked if the roof is white. Ms. Nordine explained that the roof is a mansard style which shields the flat roof on the structure. The flat roof would have a white membrane to reflect the sun rather than absorbing. This cannot be seen since it is behind the mansard roof.

Commissioner Ferrington noted the median planned on Victoria near the intersection with Owasso Street. She asked if there would be a cross walk there, so that pedestrians, especially children going to school, could cross one direction of traffic at a time, since the street will be wider and it is a busy intersection. However, if it is not close enough to the intersection signal lights, that may not be possible. Ms. Nordine stated that she will discuss the issue further with the Public Works Director.

Commissioner McCool stated that provided parking would be 147 stalls fewer than what are required. Although shared parking is shown with 51 additional spaces, that still does not reach the number required. Ms. Nordine responded that she looks at the apartment complex as a whole, which has about 875 stalls.

Chair Solomonson asked if there would be a problem with snow sliding from the roof. Ms. Nordine stated that the roof is a standard seam and can be designed with snow breaks.

Chair Solomonson called a five-minute recess after which he reconvened the meeting.

City Attorney Filla stated that the required notices have been published and provided for this public hearing.

Chair Solomonson declared the public hearing open.

Mrs. Murt Seltz, Owasso Heights Road, stated there are a lot of children in the neighborhood. The building will be an eyesore and a big six-story building is like fitting a square peg into a round hole. It is too big. She is not sure if tax money is being used. She does not believe that "Building it they will come." Big projects get built that do not work out as planned, such as Galtier Plaza and River Place. Luxury apartments and trains do not belong in the same sentence. People who can afford luxury apartments will have choices and will not want to hear trains. In the last week, two trains went through backing up traffic during school dropoff time. On many levels this plan does not work. The last thing she would want is a huge economic dinosaur on that corner that compromises safety.

Ms. Carrie Lemay, 530 Lake Cove Court, asked if the school district has addressed the capacity of the school and the traffic at that corner. As a parent of children in Island Lake School, it is bursting at the seams. She is very concerned about the added number of children that may attend that school. The school is tearing down walls and trying to find new spaces to be made into

classrooms because it is at capacity. Also, she asked if the gathering space planned in the landscaping would become a smoking lounge, if there is no smoking in the building. That area would be located right across the street from the elementary school where children would observe what is going on. She would like to know the purpose of that space.

Ms. Patricia Gunderson, 390 Harriet Avenue, agreed with what the first speaker said. If the roof shown is not accurate, is something else not accurate. The six-story building does not fit in the neighborhood. She is also concerned about traffic.

Mr. Rolly Seltz, stated that he is a volunteer in the Island Lake School. At 8:15 and 8:30, when children arrive traffic is already a mess with little children climbing out of cars with one and two back packs. This would add to the problem of traffic and children. In the afternoon, it is even worse.

Mr. Noah Bly, Urban Works Architecture, introduced the development team, Max Segler and Alan Menning from Tycon and Civil Engineer Dan Tilson from G-Cubed. He stated that the height is intrinsic to making this project work. A larger setback has been created from Victoria with a buffer area where there will be a gazebo and area for grills. It is not perceived as an area for smokers. The building will be smoke free. The building was also moved further south. This allowed a green buffer between the flat lot parking and the building, which will make the units on that side more attractive. There is also a buffer between the new sidewalk and the road. Native plantings will be used on the south side. The underground parking will be structured so that fire trucks can use the space on top. Every unit has a screened porch. The storm water will be treated before reaching Lake Shoreview. The only water that will be directly discharged into the lake is clean water from the roof.

Although those who spoke at this meeting are opposed to the building, there is also community support, as some have asked to be on the waiting list to get into the building.

Commissioner Ferrington asked the reason it is necessary to have a building of six stories. **Mr. Bly** stated that it is hard to convey the quality of the building on the exterior and interior. To make it work economically, it is a challenge to make it work without a certain scale. It has been reduced from 120 units to 104 units. If the building were five levels with wood construction, it would be almost the same height. The design of the roof will mitigate some of the impact.

Commissioner Thompson asked about the adequacy of the parking and market demand.

Mr. Bly responded that the parking is based on covering resident needs. There is a substantial amount of enclosed parking, which is what the market is interested in. The building has 138 bedrooms. The ratio used is 1.27 stalls per bedroom and 1.7 stalls per unit. Chances are the building needs fewer than 1.7. But the developer has chosen to increase that number to make sure to meet resident needs. There 124 enclosed stalls, which is more than the number of units plus the excess stalls and plus there are excess stalls around them. As for demand, there is low vacancy at this time for multi-family because there has been so little construction. There is a seismic shift in how people rent. Money that could be used to buy a house will be used for rent for a product of this type.

Chair Solomonson asked if there are common areas in the building and if there is adequate parking for a big party. He also asked for further information about the roof. **Mr. Bly** stated that there are plenty of stalls for those coming as guests plus many stalls in Midland. The roof is a mansard roof with standard metal seam. The flat portion is on the concrete deck behind the mansard roof. As for ice and snow, the roof will have pleats to stop snow and ice. There will be a canopy over the entrance so there would be no chance of snow falling on people.

Chair Solomonson asked Mr. Bly to address traffic issues. **Mr. Bly** stated that a traffic study was done. Ms. Nordine added that traffic generation for the existing land uses as well as the proposed development has been taken into consideration with the redesign of the roadways.

Commissioner McCool asked if the adjacent parking lot consists of 51 spaces. **Mr. Bly** answered that it will be reconfigured to have 51 spaces and will serve the adjacent building as well as serve as overflow to the proposed building. There is also parking around the adjacent building in addition to that lot.

Commissioner Ferrington asked if the EQC recommendations will be considered. **Mr. Bly** stated that they have just been received. They will certainly be studied and considered, but he does not know if they will be implemented.

Mr. Bly stated that the funding for the project is private. Part of the financing is with tax increment financing, which means taxes are used to pay for part of the development costs.

Ms. Patricia Gunderson, 390 Harriet Avenue, expressed concern about the occupancy rate. She asked if there has been a study in Shoreview of the need for this type of housing. What is the demand for rental? What is the occupancy rate for rental now? What will the rent be? Also, she is concerned that it will become low income housing because she is not sure people will want a luxury apartment across from two schools and a train with a busy intersection. There is enough low income housing. She does not agree that there is enough parking and still does not support a six-story building.

Mrs. Murt Seltz, Owasso Heights Road, asked who owns the pond and expressed her concern for water quality. For luxury apartments, she would guess there may be two vehicles per apartment so she is very concerned about parking. She does not understand why the building has to be so tall to be profitable. If so many variances are needed, perhaps this is not the best plan for this property. It may be too big and too high. The school is bulging at the seams. The principal has concern about how many more students can be absorbed. There are often 200 cars in passing trains, s long train.

Mr. Bly responded that Midland Terrace is currently at 98% occupancy. With turnover, that is approaching the maximum. There is a significant demand for rental units. The rental rates will be higher, perhaps \$1300 for a one bedroom apartment, which is not affordable housing. He does not have estimates on how many children there might be.

Ms. Nordine explained that lot lines to extend into the wetland area. Tycon Companies is the underlying owner of wetland/pond. The City uses it to manage stormwater runoff from the adjacent roadways.

Commissioner Ferrington asked the repercussions if this plan were not approved. Is the road linked to the development. Ms. Nordine stated that without the road realignment, the property cannot be redeveloped.

Commissioner Wenner asked if there will be recreational use of the water. **Mr. Bly** stated that the preference is for recreation. At this time, a dock is not in the plans because of the difficulty of handicapped access. The developer is interested, but a plan has not been developed.

MOTION: by Commissioner Schumer, seconded by Commissioner Wenner to close the public hearing.

VOTE: Ayes - 7 Nays - 0

Commissioner McCool asked for further information about the tax increment financing (TIF) of this project. Ms. Nordine stated that she is not familiar with those details. The developer has been working with Community Development staff. An application for TIF assistance has been submitted, which will be considered by the City Council at the June 18th meeting.

Commissioner Wenner asked if there have been discussions with the school district regarding traffic and whether it would be possible for traffic to get to the school on the other side of it. Ms. Nordine stated that discussions have occurred regarding the roadway improvements. There has not been specific discussions about the apartment land use. There are constraints to the Island Lake School site in regard to traffic flow, recreation needs and what can be done to resolve issues with the dropoff and pickup of children at that school. The back of the school is used by buses. That has eased the issue, but she does not know if there could be access in back for parents.

Chair Solomonson stated that the benefits of this project include a housing opportunity Shoreview does not have, a road realignment, storm water system, but the negative is the height of the building. With a lake on one side, a railroad and 500 feet from the nearest residential area, this is a unique area. He still has a concern about parking, but he likes where the building has been moved. The benefits outweigh the negative of the building height. He will support the proposal as presented.

Commissioner Ferrington stated that she does not want the City to lose the money captured for the road realignment, which will be a big improvement. The changes made have improved the development. She can support the proposal, even though she also does not like the height of the building.

Commissioner McCool stated that there are many positives and it does bring something that Shoreview does not have. He still has concerns about parking and is not satisfied that parking can be solved by pushing it into Midland Terrace because there is no legal right for that use.

Using Midland Terrace could be shut off at any time. The adjacent building will be losing parking. It is not excess parking but will create a problem for that building. The scale is needed for the quality. TIF will subsidize the cost in the long term. The City is spending money to be recaptured with tax increment. There is too much building on this site. Density is figured on a 6-acre site that is two-thirds water. The building far exceeds height limitations, and the development is 100 parking spaces short. There could be a better use of the site.

Commissioner Schumer stated that he is pleased to see the setback from Victoria and is not so concerned about the height. The positives outweigh the negatives, and he supports this project.

MOTION: by Commissioner Schumer, seconded by Commissioner Wenner to recommend the City Council approve the following requests submitted by Lakeview Terrace/Tycon Co. for the redevelopment of Midland Plaza, 3588 Owasso Street with a 104 unit apartment building. Said recommendation for approval is subject to the following conditions.

Comprehensive Guide Plan Amendment

1. The planned land use of the property changing the existing C, Commercial land use designation to RH, High Density Residential.
2. Review and approval of the amendment by the Metropolitan Council.
3. The amendment will not be effective until the City grants approval of the Final Plat and PUD - Final Stage requests and the development agreements are executed.

Rezoning

1. This approval rezones the property from C1, Retail Service and R3, Multi-Dwelling Residential to PUD, Planned Unit Development.
2. The underlying zoning district for this PUD is R3, Multi-Dwelling Residential
3. Rezoning is not effective until approvals are received for the Final Plat, PUD - Final Stage and development agreements executed.

Preliminary Plat

1. A public use dedication fee shall be submitted as required by ordinance prior to release of the final plat by the City.
2. The final plat shall include drainage and utility easements along the property lines. Drainage and utility easements along the roadways shall be 10' wide and along the side lot lines these easements shall be 5' wide and as required by the Public Works Director.
3. Private agreements shall be secured between the parcels in the subdivision and the adjoining Midland Terrace Apartment complex regarding joint driveway, parking and maintenance agreements. Said agreements shall be submitted to the City Attorney for review and approval prior to the City's release of the Final Plat.
4. The developer shall submit an application to vacate Owasso Street with the Final Plat application.
5. The Final Plat shall be submitted to the City for approval with the Final Stage PUD application.

Planned Unit Development – Development Stage

1. This approval permits the redevelopment of 3588 Owasso Street parcels with a 104 unit 6-story tall apartment building as depicted in the plans submitted as part of this application.

2. Private agreements shall be secured between the parcels in this PUD and the adjoining Midland Terrace Apartment Complex regarding joint driveway, parking and maintenance agreements. Said agreements shall be submitted to the City Attorney for review and approval prior to the City's review of the Final Stage PUD plans and Final Plat.
3. Access to the proposed development is prohibited from Victoria Street. Access to the site shall be provided via the driveways off of Owasso Street as indicated in the approved plans.
4. Revisions may be required to the tree preservation plan that addresses the replacement of trees along the existing Owasso Street. A revised plan, if required, shall be submitted with the Final Stage PUD and Final Plat applications. Replacement trees shall be planted in accordance with the City's Woodlands and Vegetation Ordinance.
5. Approval of the final grading, drainage, utility, and erosion control plans by the Public Works Director is required, prior to submittal to the City of applications for Final Plat and PUD – Final Stage. Final plans shall identify site construction limits and the treatment of work (i.e. driveways, parking areas, grading, etc.) at the periphery of these construction limits.
6. The proposed apartment housing structure shall be of a 6-story design as depicted on the plans submitted with this application. Said building shall include the architectural enhancements and high-quality building materials as identified. The structure shall not exceed the height, 78.5' as identified in this report and on the submitted plans.
7. Additional landscaping may be required along the south side of the building to soften the structure's appearance when viewed from the adjacent single-family residential neighborhood.
8. The applicant is required to enter into a Site Development Agreement and Erosion Control Agreement with the City. Said agreements shall be executed prior to the issuance of any permits for this project. The Development Agreement shall address:
 - a. Construction management and nuisances that may occur during the construction process, including parking for contractors. No parking is permitted on Victoria Street.
 - b. Best Management Practices for Water Quality improvement
 - c. Landscape maintenance
 - d. Maintenance of stormwater management facilities, including the filtration basins
9. This approval shall expire after two months if the Planned Unit Development - Final Stage application has not been submitted for City review and approval, as per Section 203.060 (C)(6).

This approval is based on the following findings:

- 1) The proposed redevelopment plan supports the policies stated in the Comprehensive Plan related to land use, housing and redevelopment.
- 2) The proposed redevelopment plan carries out the recommendations as set forth in the Housing Action Plan
- 3) The proposed redevelopment plan will not adversely impact the planned land use of the surrounding property.
- 4) The proposed deviations permit this site to be redeveloped with a use that expands life-cycle and affordable housing, including housing choice in the city. The plan also results in a public road improvement project that will improve traffic flow and safety.

- 5) Sustainable design features will be incorporated into the building and site design.
- 6) Certain conditions as detailed in the Development Ordinance exist.

VOTE: Ayes - 5 Nays - 1 (McCool)

MISCELLANEOUS

City Council Meetings

Commissioners McCool and Schumer will respectively attend the May 7, and May 21, 2012 City Council meetings.

Planning Commission Workshop

The Planning Commission will meet in a workshop session immediately prior to the regular May 22, 2012 meeting, at 6:00 p.m.

Text Amendment - Temporary Signs

Ms. Nordine reported that the proposed amendment is in response to the City Council requesting staff to review the ordinance on temporary signs. The main issues of enforcement relate to temporary signs and message center signs. Further, the amendment would streamline the review process for applicants. The maximum sign area would be increased for larger buildings, such as Target; increasing the number of times per year temporary signs can be used; and increasing the time of display for temporary signs from 7 to 14 days.

The need for a Comprehensive Sign Plan would be eliminated, if signs are in compliance with the City's development code. A Comprehensive Sign Plan would be required when there is deviation from the ordinance.

Chair Solomonson asked Commissioners for general comments in response to more leniency with temporary signs and use of message centers. He commended staff for incorporating previous Planning Commission feedback. In regard to using the message centers to alleviate the need for temporary signs, his concern would be that the message center is readable from the road.

Commissioner Proud stated that he strongly disagrees with broadening the use of message center signs. He has seen no evidence or statistics that would show that it would add value to the business community. They do detract from the aesthetics of the community. Further, he stated that there is no ability to measure the brightness of the message center signs. He would not want to liberalize their use without the ability to measure brightness and enforce code regulations. He would like to see this matter brought to a workshop discussion.

Commissioner Wenner noted that in the City there is a proliferation of banners and sign boards that are in violation of the current ordinance. There is an issue of enforcement of standards,

which is more important to him than type of signage. If there is going to be a change to the ordinance, there needs to be the ability for enforcement.

Chair Solomonson stated that he would favor added flexibility for use of message centers, which he believes look better than a variety of temporary signs and banners. He agreed that enforcement is also an issue. He asked what feedback has been given by the Economic Development Commission (EDC). Ms. Nordine responded that the EDC is in general in favor of more flexibility in temporary sign regulations and also with message centers. Businesses feel that competitive businesses in other communities have an edge in advertising. She added that the City permit process is cumbersome.

Commissioner McCool agreed that this is mostly an enforcement issue. He would like to see the ordinance stipulate specifically what is acceptable, but whether more flexibility or not, it needs to be balanced with enforcement. Ms. Nordine stated that enforcement does consist of sign sweeps. More often it is in response to complaints.

Chair Solomonson suggested a joint workshop with the Economic Development Commission and the City Council because he is not sure the Planning Commission has all the information on this issue.

ADJOURNMENT

MOTION: by Commissioner Schumer, seconded by Commissioner Ferrington to adjourn the regular Planning Commission Meeting of April 24, 2012, at 10:12 p.m.

VOTE: Ayes - 7 Nays - 0

TO: Planning Commission
FROM: Kathleen Nordine, City Planner
DATE: May 17, 2012
SUBJECT: File No.2448-12-11, Comprehensive Sign Plan Amendment, Signcrafters/Target Corporation, 3800 Lexington Avenue

Introduction

Target Corporation is changing their brand logo for their Super Target stores and has submitted an application to amend the existing Comprehensive Sign Plan to install signs consistent with the new brand logo. The proposal includes replacing the existing Super Target wall signs located on the north and west building walls with new wall signs and refacing the existing monument sign.

Site History

In 1989, Target Corporation approached the City with plans to develop a retail center that included a Target retail facility and a grocery store. These plans were approved via the Planned Unit Development (PUD) process. The PUD was approved using the regulations of the C-1, Retail Service district as a base. In addition, other performance standards requiring interior dumpster locations, no outside storage, and screened cart storage were conditions of approval. Target Corporation submitted revised plans in 1990 that included an expanded area for the Target retail store and set aside the area north of the building for future development. The site was developed in accordance with these plans and consists of a Target Greatland Store that has a gross floor area of 144,000 square feet.

An expansion of their facility which changed the operations from a Target Greatland to a SuperTarget, with a grocery component, was approved in 2000. Target recently reinvested in this store with a remodeling project that upgraded the interior of the facility to their current standards. The proposed signage will update the signage in accordance with their new brand logo.

Comprehensive Sign Plan

Target Corporation is seeking approval of a comprehensive sign plan to replace two existing wall signs and reface the existing monument sign along Lexington Avenue.

1. Freestanding sign: Replacing the existing monument two-sided Super Target sign panel with a SuperTarget sign panel that represents the new brand logo. The sign area (285 square feet) and height (28') will remain the same.
2. Wall Signs: Removing the existing Super Target identification signs on the West and North Building Walls and replacing them with signs that represent the new brand logo. The sign area will increase from 478 square feet to 902 square feet.

The amount of wall signage on both building walls exceeds the maximum area permitted. In accordance with the Sign Code, the sign area on a building wall is limited to 5% of the total building

elevation but not less than 40 square feet and no greater than 500 square feet. The following table summarizes the code requirements, the proposed sign area and deviation.

	Maximum Area Permitted (5%)	Existing Sign Area (% of Building Wall)	Proposed Sign Area (% of Building Wall)
West	809.6 square feet	747 (4.6%)	1,171 square feet (7.2%)
North	486 square feet	476 square feet (4.9%)	902 square feet (9.3%)

Deviations from the Sign Code requirements can be obtained through the comprehensive sign plan process. While the proposed signage is larger, it does not appear to be excessive due to the mass of the building and setback from Lexington Avenue and Red Fox Road. For these reasons, staff is supportive of the change.

Public Comment

Property owners within 350' were notified of this request. One comment was received in support of the proposal.

Recommendation

The proposed sign change is due to a change in SuperTarget’s branding. While the proposed signage exceeds the maximum area permitted per building elevation, the mass of the building necessitates signage larger than permitted. The proposed signage will not appear to be out of scale for the building. Staff is recommending approval of the proposed signage to the Planning Commission, subject to the following:

Comprehensive Sign Plan

1. Approval for the monument sign includes replacing the existing “SuperTarget” sign panel with a sign panel identifying the new brand logo for “SuperTarget”. There shall be no increase in sign area, width or height.
2. Replacement of the existing “SuperTarget” wall signs on the north and west building elevations with the “SuperTarget” wall signs as identified in the submittal.
3. Sign permits shall be obtained before the installation of any new signage on the property.

Attachments

1. Location Map
2. Applicants Description and submitted plans
3. Request for Comment
4. Proposed Motion

t:\2012pcf\2448-12-11target\preport

Target - 3800 Lexington Avenue



DISCLAIMER: This map is neither a legally recorded map nor a survey and is not intended to be used as one. This map is a compilation of records, information and data located in various city, county, state and federal offices and other sources regarding the area shown, and is to be used for reference purposes only.

SOURCES: Ramsey County (April 2, 2012). The Lawrence Group; April 2, 2012 for County parcel and property records data; April 2012 for commercial and residential data; April 2009 for color aerial imagery; All other

4/26/12
Kathleen Nordine
City of Shoreview
4600 Victoria Street North
Shoreview, MN 55126

Re: Target 3800 Lexington Ave N

To whom it may concern:



Signcrafters and Target Corporation are requesting a comprehensive sign plan review and approval for a series of new wall signs and new faces in the existing pylon sign. These signs will be in excess of the allowed signage for this property. The reason for this request is the change of the brand logo for Super Targets across the country. Plans and specifications are attached thank you for your consideration.

Sincerely,



Patrick Lawrance



2405 ANNAPOLIS LANE N, SUITE 200
MINNEAPOLIS MN 55441
P. (763) 571-2995 F. (763) 571-3588

WWW.SCOUTDOOR.COM



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TARGET

Shoreview
3800 Lexington Avenue N
Shoreview, MN
03/16/2012 | 23665 | TGT_23665_12A



EXISTING VIEW



PROPOSED VIEW



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address: 5107 Kissell Avenue
Altoona PA 16601
telephone: 814.949.8287
fax: 814.949.8293
web: blaircompanies.com

project information

client: TARGET
address: Shoreview
3800 Lexington Avenue N
Shoreview MN
store #
m number: 23665
date: 03/02/2012
rendered: MA
file name: 23665_12

revisions

- a. 03-16-12: JO: Revise pylon sign to make it larger logo, change logo to 12' logo and 9' target letters.
- b.
- c.
- d.
- e.

These drawings are not for construction. The information contained herein is intended to express design intent only.

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FRONT ELEVATION - NORTH

① 12'-0" LOGO W/ 9'-0" RED ILLUMINATED CHANNEL LETTERS - PRIMARY SIGNAGE

SCALE: NTS

Page 2 of 7





EXISTING VIEW



PROPOSED VIEW



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FRONT ELEVATION - WEST

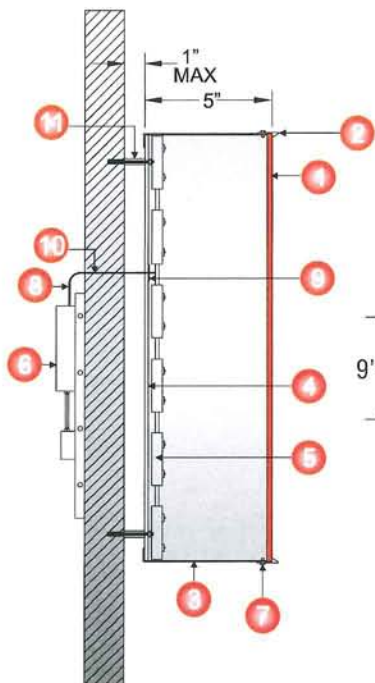
1 12'-0" LOGO W/ 9'-0" RED ILLUMINATED CHANNEL LETTERS - PRIMARY SIGNAGE

SCALE: NTS

Page 3 of 7



9'-0" ILLUMINATED CHANNEL LETTERS & 12'-0" BULLSEYE LOGO



EXTERIOR SIGNAGE

SCALE: 1/16" = 1'-0"

SECTION VIEW

1. 3/16" thk. ACRYLIC FACE
2. .090" x 1" RETAINER
3. .063" ALUMINUM RETURNS
4. .063" ALUMINUM BACKS
5. SLOAN RED LEDs
6. POWER DISTRIBUTION BOARD
7. FACE SCREWS
8. BELDON CABLE
9. PLASTIC BUSHING
10. SEALANT
11. ALL-THREAD
12. PVC COVER

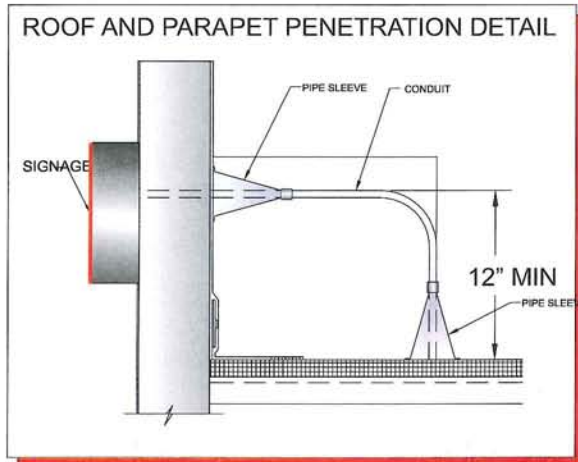
COLOR SCHEDULE:

- A. ACRYLIC FACE: RED TO MATCH LD2793
- B. RETAINER: RED GLOSS MP03909
- C. METAL LETTER: RED GLOSS MP03909
- D. LED: RED

1. 3/16" thk. ACRYLIC FACE
2. 1" TRIM CAP
3. .040" ALUMINUM RETURNS
4. .063" ALUMINUM BACKS
5. SLOAN RED LEDs
6. POWER DISTRIBUTION BOARD
7. FACE SCREWS
8. BELDON CABLE
9. PLASTIC BUSHING
10. SEALANT

COLOR SCHEDULE:

- A. ACRYLIC FACE: RED TO MATCH LD2793
- B. TRIM CAP: RED GLOSS MP03909
- C. METAL LETTER: RED GLOSS MP03909
- D. LED: RED



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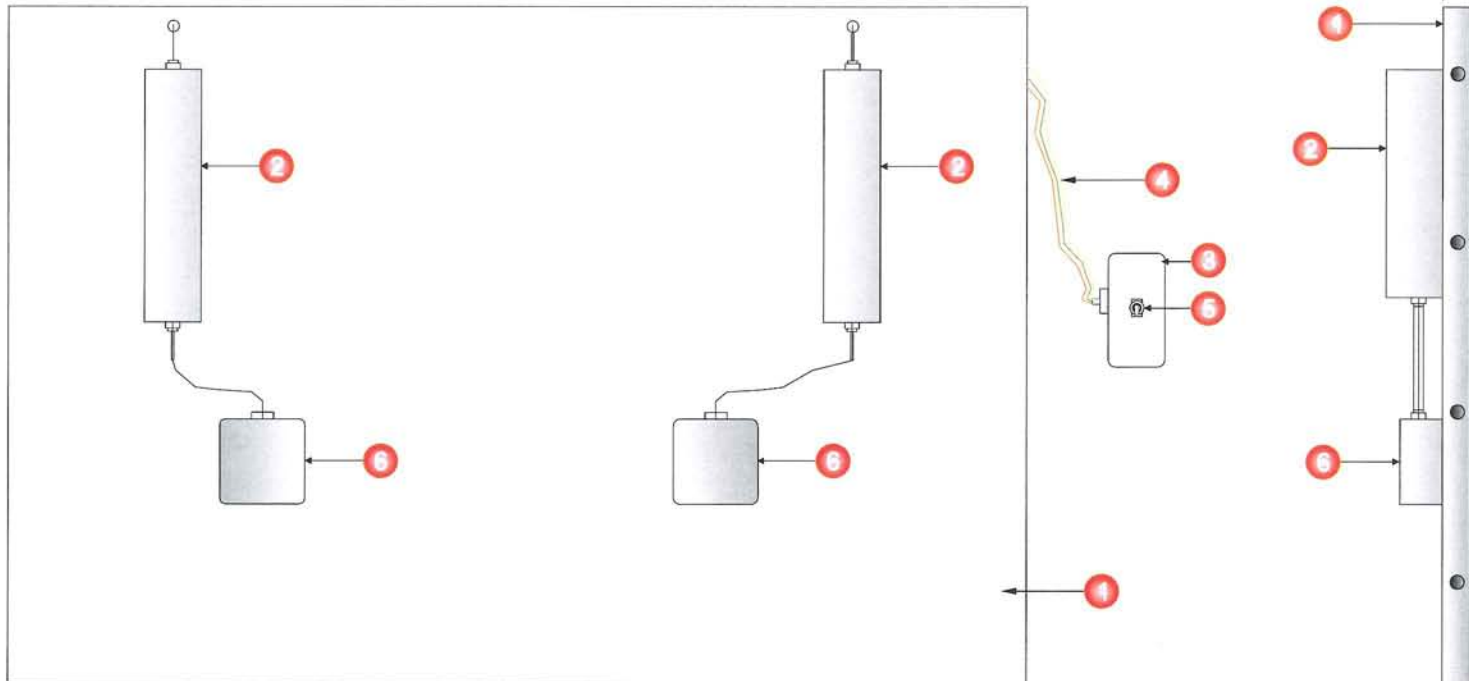
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NOTE: POWER DISTRIBUTION BOARD WILL BE USED TO POWER EACH GROUPING OF SIGNS. GROUPING DEFINED AS SIGNAGE LESS THAN 100' FROM EACH OTHER. MAX DISTANCE FOR LED DRIVERS FROM SIGNAGE GROUP IS 150'. POWER SUPPLIES WILL BE MOUNTED TO THE ALUMINUM PANEL WITH # 10-24 THREADED INSERT.



POWER DISTRIBUTION BOARD

1. 1/8" thk. ALUMINUM PANEL
2. 12 VOLT SLOAN POWER SUPPLY
* NUMBER OF POWER SUPPLIES VARY
W/EACH DESIGN
3. 4" HANDY BOX - BY ELECTRICAL CONTRACTOR
4. NON-METALLIC CONDUIT - BY SIGN VENDOR
5. 20amp DISCONNECT SWITCH - BY ELECTRICAL CONTRACTOR
6. JUNCTION BOX - BY ELECTRICAL CONTRACTOR



EXISTING VIEW



PROPOSED VIEW



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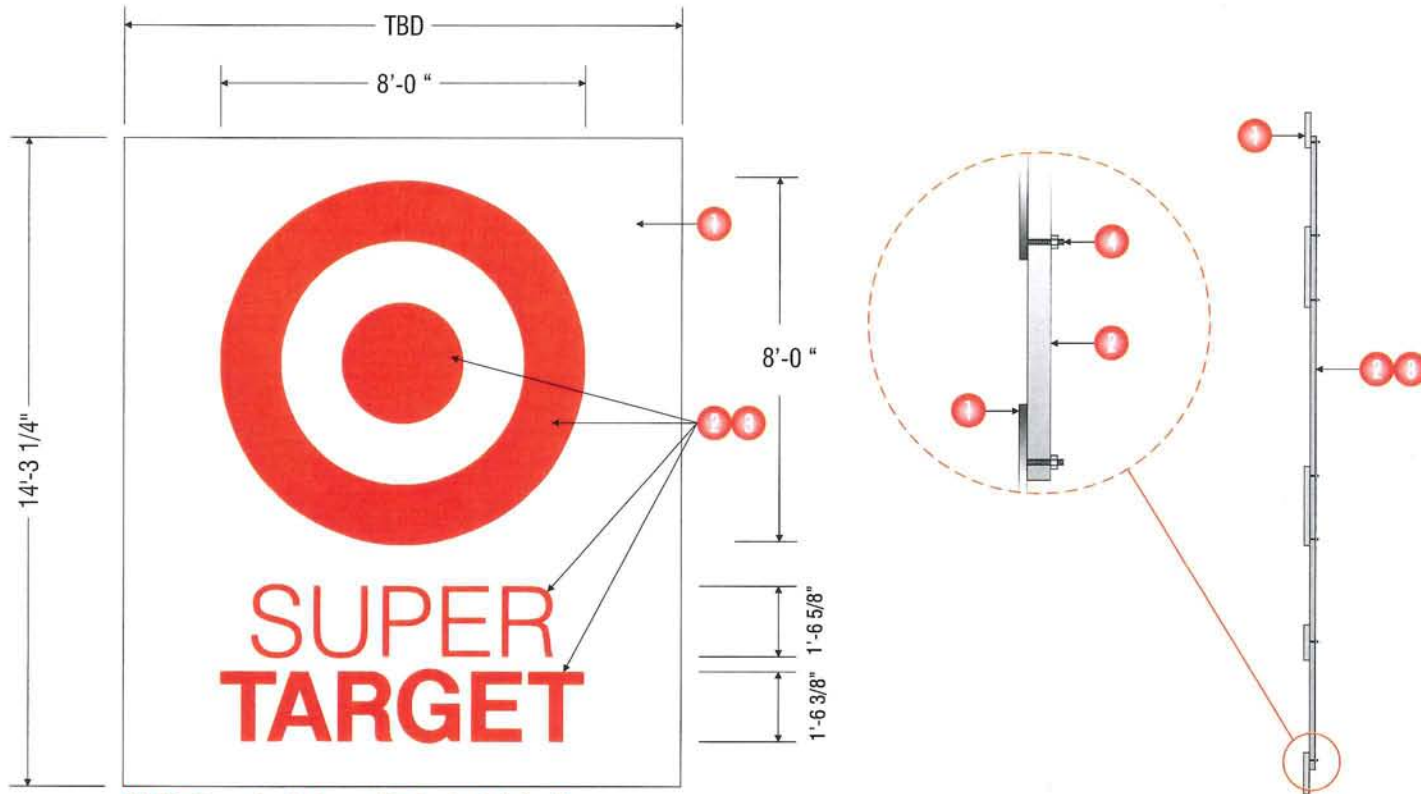
PYLON

② 14'-3 1/4" X 15'-0" PANAFLEX REPLACEMENT FACE

SCALE: NTS



14'-3 1/4" X (TBD) ROUTED ALUMINUM W/ BACKER PANEL



NOTE: Face size to be verified prior fabrication.

ROUTED REPLACEMENT FACE w/ BACKER

SCALE: 1/4" = 1'-0"

1. .125" ALUMINUM PANEL
(WHITE OF BULLSEYE IS ALUMINUM STUDDED TO THE BACKER)
2. .177" thk LEXAN BACKER
3. VINYL GRAPHICS
4. 1/4"-20 x 1" STUD w/ NUT (TO FASTEN) WELDED TO ALUMINUM

COLOR SCHEDULE:

- A. ALUMINUM PANEL: WHITE
- B. LEXAN BACKER: CLEAR
- C. VINYL: RED #3630-53 CARDINAL RED
APPLIED 2nd SURFACE 1st LAYER
- D. VINYL DIFFUSER APPLIED 2nd SURFACE 2nd LAYER

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361'
ENTIRE SIDE



Super **TARGET**

28'





553'

28'



Kathleen Nordine <knordine@shoreviewmn.gov>

Request for comment comprehensive sign plan amendment 3800 Lexington Avenue

1 message

David Carland <DCarland@stonehenge-usa.com> Wed, May 16, 2012 at 2:55 PM
To: "KATHLEEN NORDINE (KNORDINE@SHOREVIEWMN.GOV)" <KNORDINE@shoreviewmn.gov>
Cc: Randy Rauwerdink <RRauwerdink@stonehenge-usa.com>

Kathleen, per the Request for comment that we received from the City we want to go on the record as supporting the request by Target for enhanced wall and monument signage. The size of the signs seem reasonable given the site orientation and the size of the Target Store.

We do not plan on attending the public hearing but please communicate our support to the Planning Commission.

Please note our new office location effective January 4, 2012

Dave Carland
Stonehenge USA
6007 Culligan Way
Minnetonka, MN 55345
O: 952.288.2202
C: 612.963.9107
dcarland@stonehenge-usa.com
www.stonehenge-usa.com



MOTION

MOVED BY COMMISSION MEMBER: _____

SECONDED BY COMMISSION MEMBER: _____

To recommend the City Council approve the Comprehensive Sign Plan for Target, 3800 Lexington Avenue North. The proposed signage is consistent with the rebranding of Target and will not appear to be out of scale due to the mass of the building and setback from Lexington Avenue and Red Fox Road. Approval is subject to the following conditions:

1. Approval for the monument sign includes replacing the existing "SuperTarget" sign panel with a sign panel identifying the new brand logo for "SuperTarget". There shall be no increase in sign area, width or height.
2. Replacement of the existing "SuperTarget" wall signs on the north and west building elevations with the "SuperTarget" wall signs as identified in the submittal.
3. Sign permits shall be obtained before the installation of any new signage on the property

VOTE:

AYES:

NAYS:

Regular Planning Commission Meeting
May 21, 2012

TO: Planning Commission
FROM: Kathleen Nordine, City Planner
DATE: May 17, 2012
SUBJECT: File No. 2447-12-10, Site and Building Plan Review, St. Odilia Catholic Church, 3495 Victoria Street N

INTRODUCTION

The applicant, St. Odilia Catholic Community, 3495 Victoria Street N, is requesting site and building plan review for the construction of a playground. The playground would be located adjacent to the southeast corner of the building and would be used for the pre-school program. This application was complete as of May 4, 2012.

BACKGROUND AND PROJECT DESCRIPTION

The project site is located south of Island Lake Elementary School, west of Victoria Street, east of Vivian Avenue and north of Cottage Place. The property has a lot area of 14.85 acres and existing access from both streets. The site is used as a church and a private school. Recreational facilities for the school include a playground and soccer field. A single-family residence is located on Victoria Street between Island Lake Elementary School and the subject property. In addition, single-family homes are located west of Vivian Avenue.

The applicant is proposing to construct another playground located at the southeast corner of the building for the preschool program. The existing playground contains equipment for elementary school age children and not suitable for preschoolers. The proposed playground is 3,600 square feet in area and designed exclusively for preschoolers to serve the needs of the students/program better. The playground area includes play equipment and a grass play area. The area will be enclosed with a 4' tall chain link fence. Please see the attached plans.

COMPREHENSIVE PLAN

The *2000 Comprehensive Plan* designates this property for Institutional use. Uses within this category include public and private schools, fire and police stations, city hall, water towers and other public or quasi-public uses. Surrounding planned land uses include low-density residential, institutional and park.

DEVELOPMENT ORDINANCE REQUIREMENTS AND REVIEW

The property is zoned R-1, Detached Residential, a typical zoning for church and school properties. Public and quasi public uses are allowed in this zoning district through the Site and Building Plan Review process provided the use will not conflict with or impede the use of adjoining property. Conditions may be attached to site and building plan approval by the City Council to ensure compatibility with adjacent land uses.

Staff has reviewed the plans in accordance with planning and zoning requirements. Generally, the proposed improvements are consistent with the policies of the *Comprehensive Plan* and the Development Regulations. The playground is intended to serve students in the preschool program by providing active outdoor play space. This area will also be used for learning

opportunities. The proposed location south of the building exceeds the minimum setbacks required from a property line. The closest single family residential property is about 95 feet away from the playground area. A service drive for the church separates the playground area from the nearby single-family residential land uses. In Staff's opinion, the playground will not impact these residential uses.

REQUEST FOR COMMENT

Property owners within 350 feet were notified of the applicant's request. No comments were received.

STAFF RECOMMENDATION

The proposed plans meet the requirements of the City's development regulations and the intent of the *Comprehensive Plan*. Therefore, staff recommends that the City Council approve the submitted site and building plans, subject to the following conditions:

1. The project must be completed in accordance with the submitted site and building plans. Any significant changes to these plans, as determined by the City Planner, will require review and approval by the Planning Commission and the City Council.
2. The approval will expire after one year if the required permits have not been issued and work has not begun on the project.
3. The applicant shall obtain a grading permit and a fence permit for the playground and fence that will be installed around the perimeter of the playground area.

Attachments

- 1) Location Map
- 2) Motion Sheet
- 3) Submitted Plans
- 4) Applicant's statement

St. Odilia 3495 Victoria Street



DISCLAIMER: This map is neither a legally recorded map nor a survey and is not intended to be used as one. This map is a compilation of records, information and data located in various city, county, state and federal offices and other sources regarding the area shown, and is to be used for reference purposes only.

SOURCES: Ramsey County (April 30, 2012). The Lawrence Group: April 30, 2012 for County parcel and property records data; April 2012 for commercial and residential data; April 2009 for color aerial imagery; All other

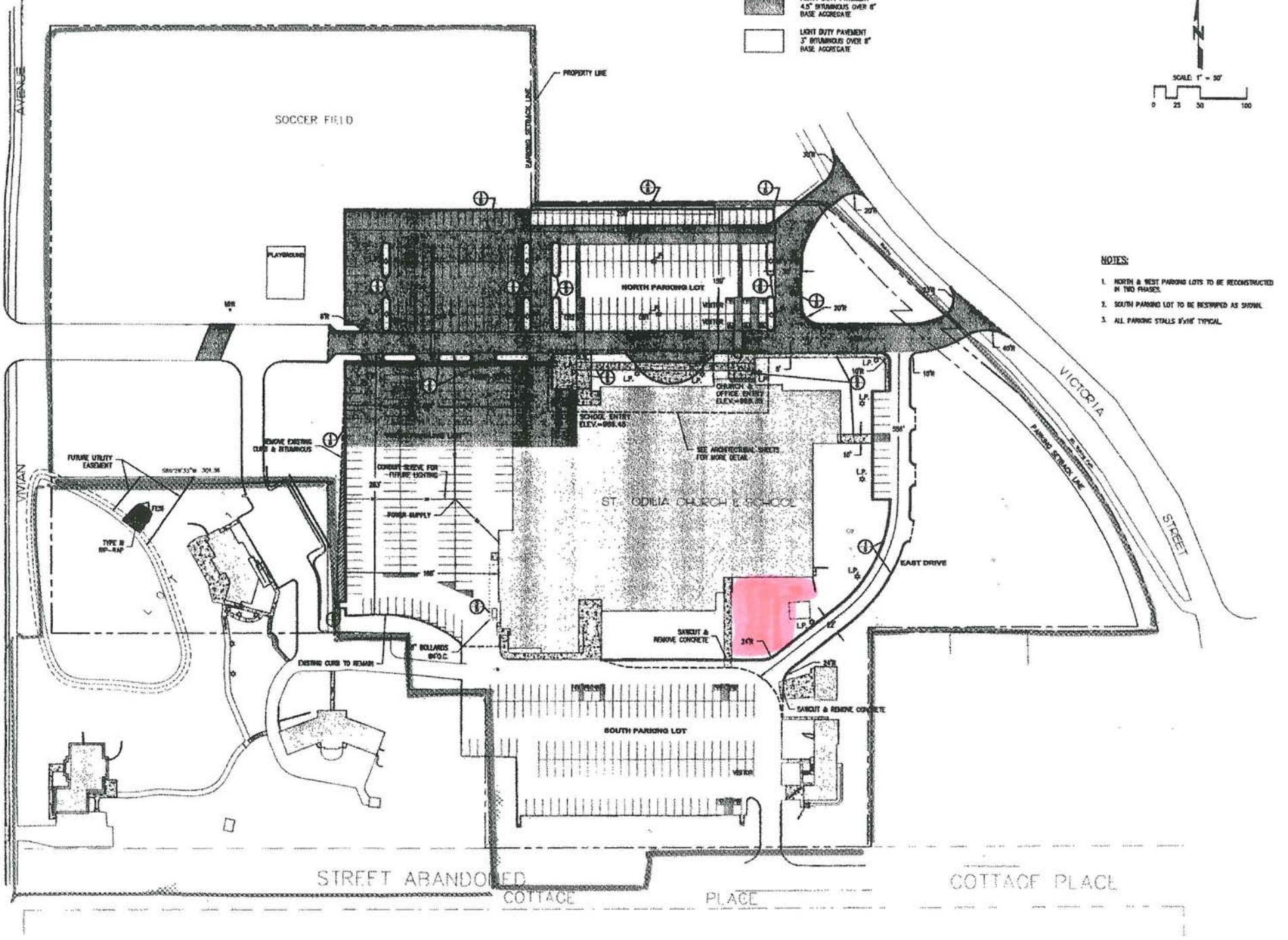
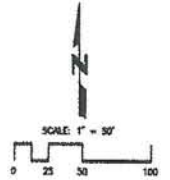
Proposed Development

Preschool Playground for St. Odilia – age appropriate for ages 2-5

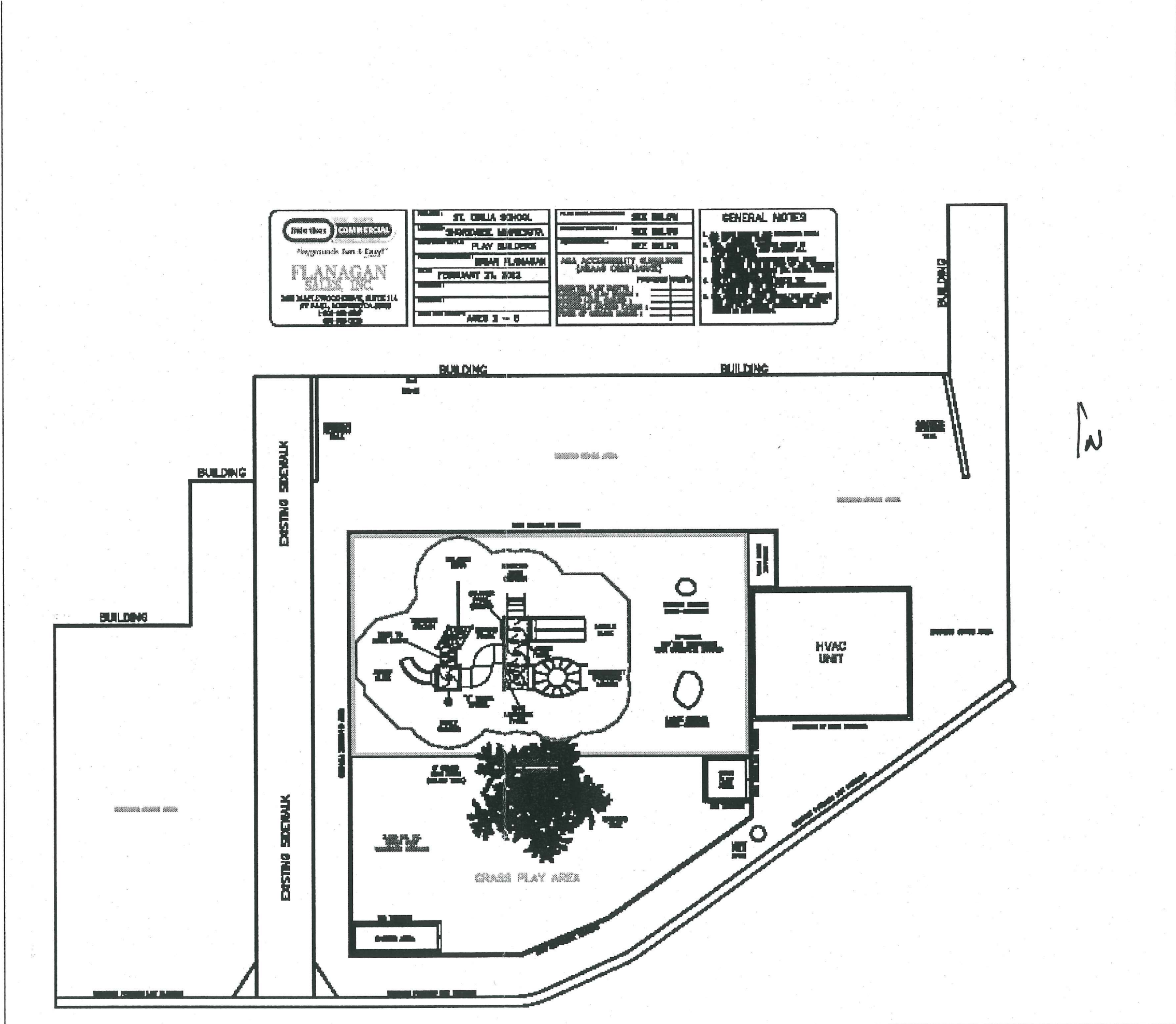
- St. Odilia Wee Saints Preschool has obtained a quote and drawings of a playground suitable for ages 2-5 from Flanagan Sales, Inc.
- The current playground on the property is built for school age children and is not age appropriate for the younger preschoolers, therefore causing safety concerns. The current playground is also used by the school children throughout the day and offers very few times available for the preschool.
- The new playground is expected to be located next to the church building on the southeast corner of the building.
- The playground will include a play structure but will also serve as an outdoor classroom with play learning activities such as a garden area and sand play area.

HEAVY DUTY PAVEMENT
4.5" BITUMINOUS OVER 8"
BASE AGGREGATE

LIGHT DUTY PAVEMENT
3" BITUMINOUS OVER 8"
BASE AGGREGATE



- NOTES:**
1. NORTH & WEST PARKING LOTS TO BE RECONSTRUCTED IN TWO PHASES.
 2. SOUTH PARKING LOT TO BE RESURFACED AS SHOWN.
 3. ALL PARKING STALLS 8'x16' TYPICAL.



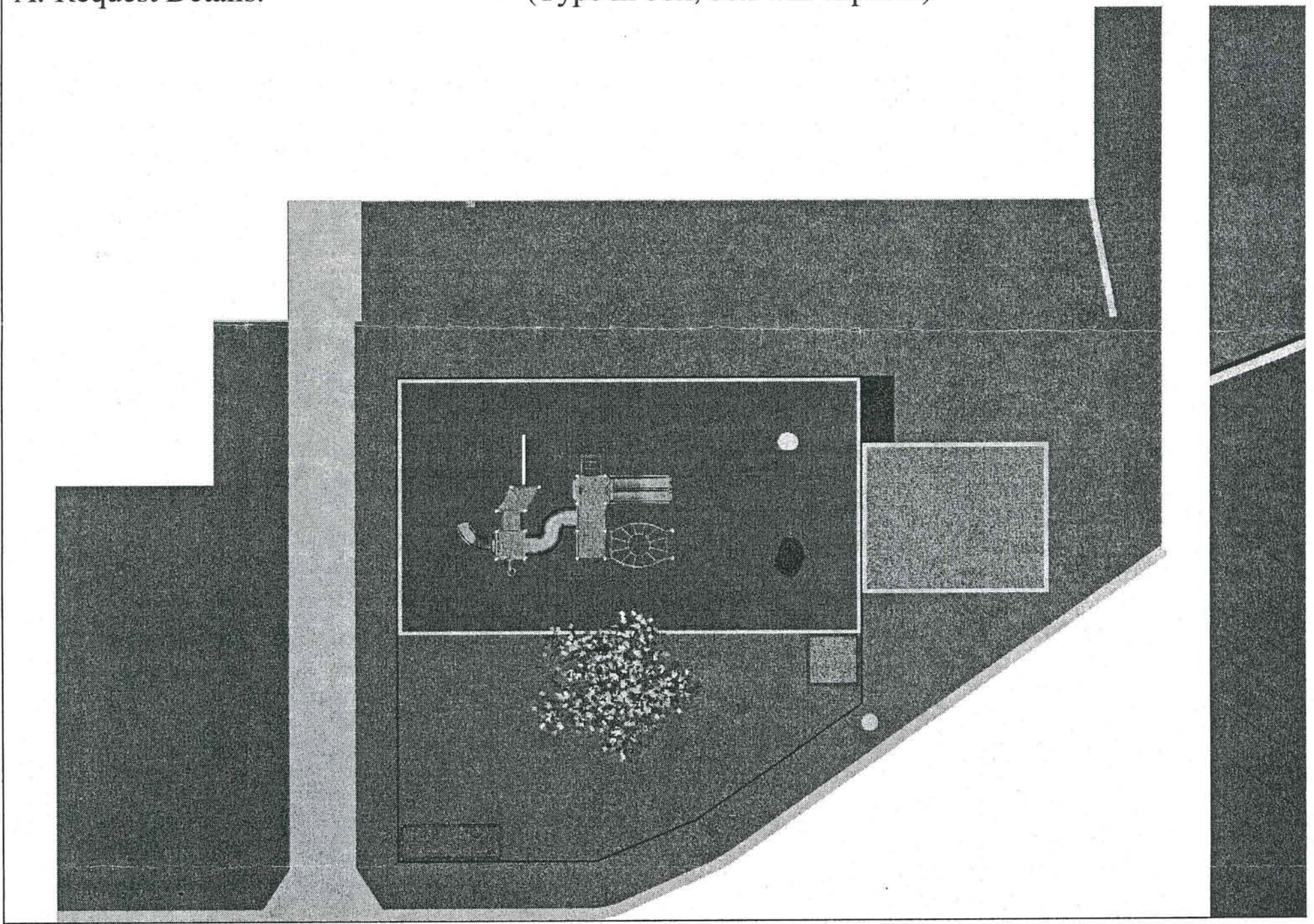
B. Tell us why this is needed / desired.
 Our preschool program has a 100 children . The equipment that they play on is not age appropriate. I

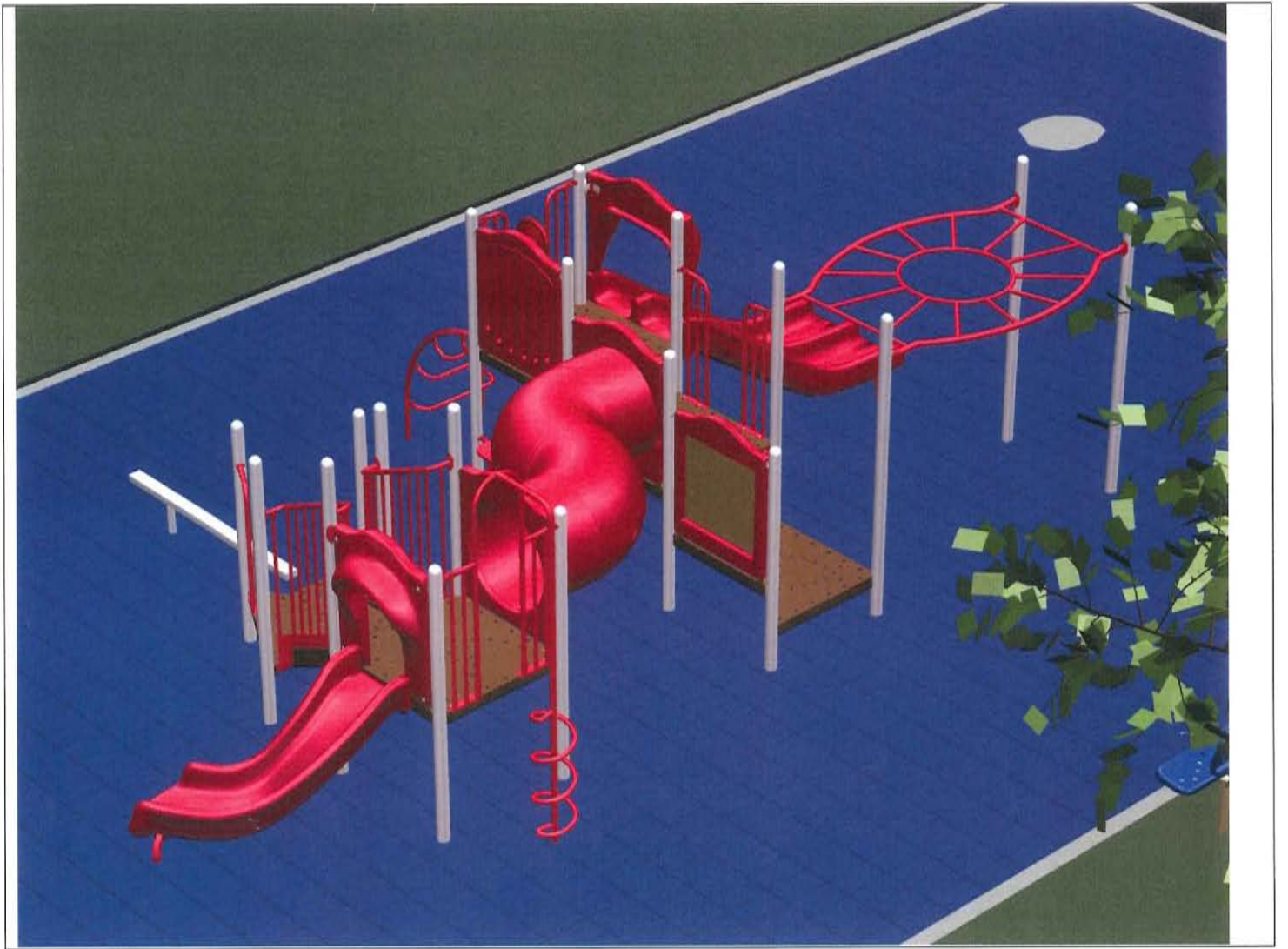
Request for Capital Improvement on St. Odilia Campus

1. Return form to Parish Administrator
2. Include details about the project (attach drawings and cost estimates as needed)
3. Include contact information in case of questions

A. Request Details.

(Type in box, box will expand.)





MOTION

MOVED BY COMMISSION MEMBER: _____

SECONDED BY COMMISSION MEMBER: _____

To recommend the City Council approve the Site and Building Plan review application for St. Odilia 3495 Victoria Street North, to construct a playground on the property for the preschool program, subject to the following conditions:

1. The project must be completed in accordance with the submitted site and building plans. Any significant changes to these plans, as determined by the City Planner, will require review and approval by the Planning Commission and the City Council.
2. The approval will expire after one year if the required permits have not been issued and work has not begun on the project.
3. The applicant shall obtain a grading permit and a fence permit for the playground and fence that will be installed around the perimeter of the playground area.

This approval is based on the following findings:

1. The proposed improvement is consistent with the policies of the Comprehensive Plan, including the Land Use and Housing Chapters.
2. The proposed improvements will not conflict with or impede the use of adjoining property.

VOTE:

AYES:

NAYS:

Regular Planning Commission Meeting
May 16, 2012

TO: Planning Commission
FROM: Kathleen Nordine, City Planner
DATE: May 17, 2012
SUBJECT: File No. 2449-12-12, Amendment to PUD, TCF Bank/Kroona – 3836 Lexington Avenue

Introduction

TCF Bank submitted an application to amend the Planned Unit Development previously approved for the Sinclair property at 3836 Lexington Avenue. TCF is proposing to redevelop the site by demolishing the existing fuel service station and associated improvements and constructing a new bank facility. This property is part of the Planned Unit Development previously approved for Target; therefore, an amendment is needed to the PUD for the redevelopment.

Site History

In 1989, Target Corporation approached the City with plans to develop a retail center that included a Target retail facility and a grocery store. These plans were approved via the Planned Unit Development (PUD) process. The Sinclair site was included in this PUD to address access issues associated with both properties. In addition, the owner of Sinclair provided additional right-of-way for Red Fox Road in exchange for flexibility from the City's regulations pertaining to signage.

Project Description

The property is located on the southeast corner of Red Fox Road and Lexington Avenue and has an area of 34,168 square feet and a width of 167' along Lexington Avenue. The applicant is proposing to demolish the existing Sinclair fuel and service station and other site improvements on the property to prepare the site for redevelopment. TCF is proposing to construct a one-story 2,200 square foot bank facility with a three lane drive-through. Access is proposed from Lexington Avenue, Red Fox Road and the Target private entry driveway. As part of the previous PUD, an easement was secured with Target permitting use of the private driveway. Parking will also be provided in an off-street parking lot and stacking space will be provided at the drive-through lanes. The applicant is seeking flexibility from the City's development standards to reduce the minimum setback required for a parking area adjacent to a public road and the number of stacking spaces required for the drive-through lanes.

Planned Unit Development – Development Stage

The Development Stage of the PUD is intended to provide the City with detailed site and building plans. Development Stage PUD establishes a scheme or guideline for building design, lighting, landscaping, parking, access and signage. Any substantial changes to the previous

approvals require review and approval through the PUD process again. The key development issues are discussed below:

Red Fox Road Improvements

At their May 21st meeting, the City Council will consider authorizing of a feasibility study for the reconstruction of Red Fox Road. Red Fox Road serves as a primary access road to this commercial area and the Island Lake Golf Course. Traffic flow and congestion issues were previously raised by the City Council and Planning Commission during review of the Stonehenge Retail Center which is currently under construction. Additional commercial development along this roadway and increased traffic may exacerbate these issues. The intent of the feasibility study is to evaluate the existing traffic conditions, street and utility infrastructure, future conditions and identify the road improvements needed to manage traffic along this roadway. If the Council supports the feasibility study and orders a road project, construction may begin in the Fall of 2012 or Spring of 2013.

Site Design and Access

Access to the site is proposed off of Lexington Avenue and Red Fox Road with a restricted right turn in lane and full access off of the Target private driveway. The building is oriented to the north facing Red Fox Road and is placed in the center of the property. The location of the structure exceeds the minimum building setbacks required. The drive-through facility is located on the south side of the building. The site has been designed to separate the pedestrian movements into the lobby with the drive-through movements for the bank.

The main concerns regarding the site design relate to the proposed access points off of Red Fox Road and Lexington Avenue. Currently, the site is developed with full access off of both roadways. These access points do present some conflict with traffic flow on both roadways. While preliminary designs have not been developed for Red Fox Road, the City Engineer has indicated that a median and additional turn lanes (including a right turn only lane to the Target entry drive) should improve traffic flow. The access proposed on Red Fox presents some minor conflict due to the proximity of the Target access driveway. While this is a concern, the City Engineer has indicated that this design may be acceptable provided additional easement right-of-way for the road improvements is given to the City.

Ramsey County staff has also indicated that the proposed access drive off of Lexington Avenue presents some conflict with their future plans to add a right turn lane onto Red Fox Road. Additional review of this matter is needed with Ramsey County.

Parking

Parking is provided on-site in an off-street parking area that provides 17 stalls, exceeding the minimum 11 required. Stacking space for the three drive through lanes located on the south side of the building is provided but is less than the minimum 6 spaces per lane. Two of the lanes provide space for about 5 vehicles and one has space for about 4 vehicles. TCF has evaluated drive through transactions and has determined that the proposed stacking meets the needs of this facility.

The parking area does comply with the minimum setbacks required from Lexington Avenue, the southern and eastern property lines. The applicant, however, is requesting a reduction of 8'8" from the minimum 20-foot setback from Red Fox Road and establish a setback of 11' 4" for the parking area. The proposed setback will still enable landscaping to be installed for parking lot screening. Furthermore, this setback is consistent with the setback recently approved for the parking area in the Stonehenge retail center, east of this site.

Architectural Design

The bank is designed as a one-story building oriented towards Red Fox Road with the drive-through canopy on the south side of the building. Exterior wall finishes include brick with an accent brick for visual interest. The roof is designed with a hip roof over the main building structure and a gable roof over the entryway and drive-through canopy. The structure complies with the City's design standards.

Stormwater Management

The proposed impervious surface coverage for this site is 64% and is less than the maximum 75% permitted for commercial sites. Stormwater runoff will be collected on site and routed through an in ground structure that will remove suspended solids before discharging into a catch basin on Red Fox Road. The proposed design complies with the City's stormwater management guidelines.

Public Utilities

The southern portion of the property is encumbered with a 20' drainage and utility easement that contains municipal water and sewer lines. These lines will need to be relocated further to the south in order to eliminate any conflict that may occur with the drive through facility when maintenance or repair is needed on these lines. This will require the vacation of the existing easement and the dedication of a new easement along the south property line.

Public Comment

Property owners within 350' were notified of this request. Dave Carland of Stonehenge, the developer of the nearby retail center, submitted a comment in support of the application. Comments were also received from the Lake Johanna Fire Department Fire Marshall and are attached.

Recommendation

The redevelopment of the Sinclair fuel and service station with a new bank facility supports the City's commercial land use goals regarding the reinvestment in commercial areas and providing services which support resident needs. While there are outstanding issues related to the proposed access points and public utility location, staff believes these can be resolved prior to the final stage approvals and should not impede action on this request. In the event access points are denied or additional right-of-way is needed, site plan changes may consist of shifting the improvements south and/or east towards the Target property. Staff is asking the Planning Commission to recommend approval of the amendment to the PUD subject to the following conditions:

Planned Unit Development – Development Stage

1. The property shall be developed in accordance with the plans submitted as part of this application. Revisions to the submitted plans may be permitted to respond to comments received from the City Engineer and Ramsey County representative. Said revisions shall be submitted as part of the Finals Stage PUD application.
2. The applicant shall continue to work with the City Engineer and Ramsey County representatives to resolve issues regarding road right-of-way, planned access points and future road improvements. These issues shall be addressed prior to the City's approval of the Final Stage PUD.
3. Prior to the issuance of a building permit, the applicant shall address the items stated in the memo from the Fire Marshal.
4. The applicant shall enter into a site development agreement and an erosion control agreement with the City prior to the issuance of a building permit for the project.
5. The existing public sewer and water utilities shall be relocated south of the proposed drive through facility and pavement area. The existing utility easement shall be vacated and a new easement established along the south property line.
6. A permit is required from Ramsey County for any work in the Lexington Avenue right-of-way.
7. The landscape plan shall be revised to provide a diversity of trees with a ratio of 30-20-10.
8. The applicant shall work with the City to develop a commercial gateway sign for the Red Fox Road retail area.
9. A Comprehensive Sign Plan is required prior to the installation of any signage on the property.
10. Authorize the Building Official to issue proper building permits after Final Stage approval of the PUD and execution of the development agreement.

Attachments

1. Location Map
2. Memo from City Engineer
3. Memo from Fire Marshall
4. Applicants Description and Submitted Plans
5. Request for Comment
6. Proposed Motions

t:\2012pcf\2449-12-12tcf\kroona\pcreport

3836 Lexington Avenue

TCF Bank - PUD



DISCLAIMER: This map is neither a legally recorded map nor a survey and is not intended to be used as one. This map is a compilation of records, information and data located in various city, county, state and federal offices and other sources regarding the area shown, and is to be used for reference purposes only.

SOURCES: Ramsey County (April 30, 2012), The Lawrence Group; April 30, 2012 for County parcel and property records data; April 2012 for commercial and residential data; April 2009 for color aerial imagery; All other

Date: May 18, 2012

To: Kathleen Nordine, City Planner

From: Mark Maloney, Public Works Director
Tom Wesolowski, City Engineer

Subject: PUD Development Stage Application Review Comments for the Proposed
TCF Bank – 3836 Lexington Avenue

The City of Shoreview Engineering Department has reviewed the preliminary plans and storm water management report dated April 30, 2012. The engineering staff has the following comments regarding the plans:

1. The proposed development falls within the Rice Creek Watershed District, but because the site is less than 1-acre in size a Rice Creek permit is not required. The City will act as the regulating authority for storm water management on the site.
2. The developer has submitted storm water management calculations for existing and proposed drainage. The calculations are signed by a licensed engineer from the State of Minnesota and meet the requirements of the City's Surface Water Management Plan (SWMP).
3. The proposed storm water management system will treat run off from the site and reduce the volume of flow to a level less than what currently leaves the site, which meets the requirements of the City's SWMP. Treatment will consist of an underground structure that will remove suspended solids before it is discharge into the existing storm water collection system that is located on Red Fox Road.
4. There is a storm pond located to the south of the site that is owned by Target and runoff from the site could be directed to the pond, but it would require an upgrade to the pond and an agreement with Target. At this time, the City is considering improvements to Red Fox Road to improve traffic flow, and the project would be subject to Rice Creek Watershed District requirements to treat the runoff from the road. If the Red Fox Road improvements proceed there is potential for the City, TCF, and Target to complete a joint project that would upgrade and increase the capacity of the pond, which would increase the effectiveness of water quality treatment of runoff from all properties. In that scenario, the City would assume future maintenance responsibility for pond.
5. In the event that this redevelopment proceeds without the above mentioned changes to the regional storm water treatment pond on the adjacent Target site, TCF would be required to provide a maintenance agreement to the City for their on-site storm water collection and treatment system as part of the development agreement.

6. The utility plan for the proposed redevelopment shows existing City water and sanitary sewer pipelines located under the drive-thru area and adjacent pavement. As part of the redevelopment project the City's utilities would need to be relocated to the south end of the site outside of the paved area. This would require the existing utility easement be abandoned and a new utility easement be established along the south side of the property.
7. The proposed right-in entrance off Red Fox Road appears redundant and complicates the potential future public street improvement. To improve traffic flow on Red Fox the City is planning the reconstruction of the roadway, which would include managing access entering and exiting Red Fox. The proposed entrance off of Red Fox would pose much less of access management issue to the City if the TCF redevelopment included dedication of a strip of public right of way to accommodate improved lane geometry for traffic the area.
8. The Lexington Avenue right of way is controlled by Ramsey County. A Ramsey County permit is required for any work completed in their right of way. Ramsey County officials have indicated the need for a right turn lane from northbound Lexington Avenue.
9. The PUD application will be presented to the Environmental Quality Committee for comment at their May 29th meeting.

If you have any questions or would like to discuss the comments in more detail please contact Mark Maloney or Tom Wesolowski.



LAKE JOHANNA FIRE DEPARTMENT

5545 LEXINGTON AVENUE NORTH • SHOREVIEW, MN 55126
OFFICE (651) 481-7024 • FAX (651) 486-8826

May 10, 2012

Department of Community Development
Attn: Kathleen Nordine, City Planner
4600 N Victoria Street
Shoreview, MN 55126

Site and Building Plan Review
TCF Bank
3836 Lexington Ave
Shoreview, MN 55126

File No. 2449-12-12

- Verify location of F.D.C.
 - Should be on address side of building.
- Fire Hydrant within 150' of F.D.C.
- Verify location of riser room.
- Fire Department lock box is required.
- Must maintain end drive-thru width for fire truck access.

Sincerely,

Rick Current
Fire Marshal
Lake Johanna Fire Department

9 May 2012

Ms. Kathleen Nordeen
Department of Community Development
City of Shoreview
4600 Victoria Street North
Shoreview, MN 55126



Re: TCF Bank
New Branch Bank
Shoreview, Minnesota

Dear Kathleen:

We are pleased to present our plans for redevelopment of the property at 3836 Lexington Avenue as a new TCF Bank branch. TCF is thrilled to provide a convenient opportunity for both existing and future TCF Bank customers within the community.

Although bank branch transactions have declined within the industry in recent years, drive-up banking continues to be a valuable service. In order to safely provide both drive-up banking and lobby banking on the same property, we strive to isolate the drive-up lanes from pedestrian traffic. Although this requires more space, it is essential to providing a safe, convenient site for bank customers. To accommodate these requirements, we are proposing a deviation of 8'-8" from the 20 foot minimum parking setback along Red Fox Road. Our drive lane would be 11'-4" from the property line. The current condition includes a setback of approximately 8 feet, so the proposed condition will be a smaller deviation from the existing one.

While drive-up banking is still valued, the number of drive-up transactions is also decreasing. We have designed the drive-up at this location to accommodate up to 13 vehicles. We recognize that this is a deviation from the 18 cars that are required by the zoning ordinance. Based on recent transaction information, and published recommendations on drive-up stacking, we are confident that the total stacking will be sufficient for their needs. We are concerned that by dedicating additional space to the drive-up stacking we may compromise the ability for lobby customers to safely negotiate the parking lot and find a parking space.

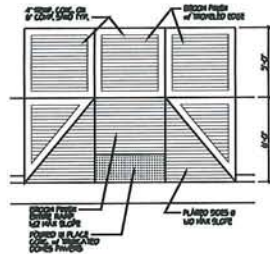
We are excited to bring this new use to the city of Shoreview, in a way that is both safe and convenient for the community. If you have any questions or comments regarding any part of this submittal, please feel free to call. I look forward to continuing to work with you on this project.

Sincerely,

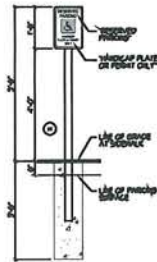
A handwritten signature in black ink, appearing to read "Michael F. Kraft". The signature is fluid and cursive, written over a horizontal line.

Michael F. Kraft
Partner
HTG Architects

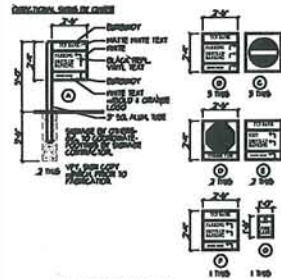
PC: Dave Nelson, TCF Bank



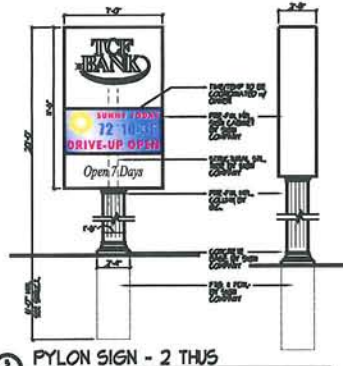
3 ADA RAMP DETAIL
1/4"-1'-0"



5 ADDITIONAL SIGN FACES
1/4"-1'-0"



4 SITE SIGNAGE
1/4"-1'-0"



2 PYLON SIGN - 2 THUS
1/4"-1'-0" (80 SQ. FT.)

KEY NOTES

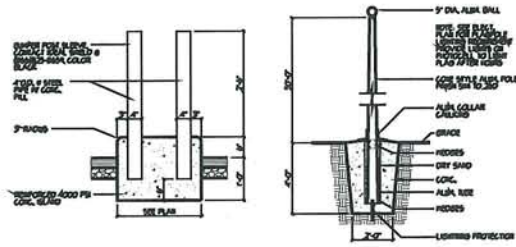
- 1 PYLON SIGN SEE S&D
- 2 COMPLETE SPECIFIC OR OTHER SIGNAGE. SEE CALL FOR DETAILS.
- 3 SIGNAGE TO BE CONSTRUCTED BY 2ND PARTY. SEE CALL FOR DETAILS.
- 4 SIGNAGE TO BE CONSTRUCTED BY 2ND PARTY. SEE CALL FOR DETAILS.
- 5 SIGNAGE TO BE CONSTRUCTED BY 2ND PARTY. SEE CALL FOR DETAILS.

GENERAL NOTES

- VERIFY & LOCATE ALL UNDERGROUND UTILITIES PRIOR TO ANY EXCAVATION.
- SEE LOCAL & STATE DEPARTMENTS FOR ADDITIONAL REGULATORY REQUIREMENTS.
- SURVEY FOR CONSTRUCTION AREA AND EXISTING SIGNAGE. SEE CALL FOR DETAILS.
- SURVEY FOR CONSTRUCTION AREA AND EXISTING SIGNAGE. SEE CALL FOR DETAILS.
- SURVEY FOR CONSTRUCTION AREA AND EXISTING SIGNAGE. SEE CALL FOR DETAILS.

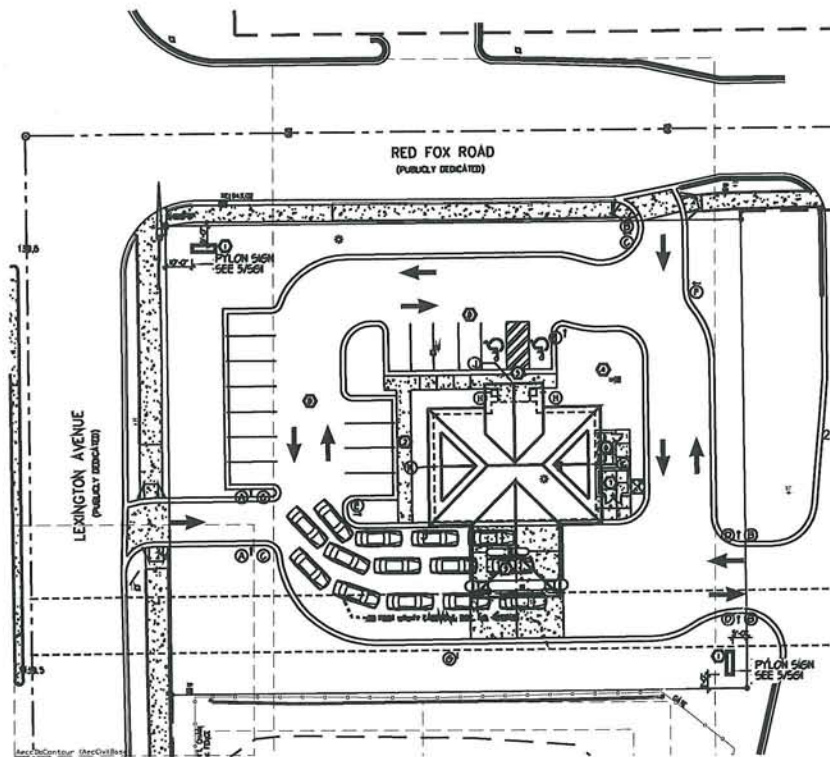


6 BUILDING SIGNAGE
1/4"-1'-0"



7 BOLLARD DETAIL
3/4"-1'-0"

8 FLAG POLE DETAIL
1/4"-1'-0"



9 SITE PLAN
1/4"-1'-0"



www.htg-architects.com
Minneapolis Phoenix Tampa

9300 Hennepin Town Road
Minneapolis, MN 55347
Tel: 952.278.8880
Fax: 952.278.8822

PROJECT

**TCF NATIONAL BANK
SHOREVIEW, MN**

NEW CONSTRUCTION

ISSUED SET	REVISIONS
DATE	NO.

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT HAS BEEN PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

MICHAEL F. KRAFT
8/23/10
SHEET NO. DATE

SITE SIGNAGE PLAN DETAILS

24-0000-0001.dwg 10/11/10 3:00 - 3:00pm

DRAWN BY: JZJ CHECKED BY: MKL



8/23 3/10/10
COPYRIGHT © BY HTG ARCHITECTS

Site Development Plans

for

TCF BANK

Shoreview, Minnesota

Presented by:

HTG ARCHITECTS



Client
HTG ARCHITECTS
 9300 HENNEPIN TOWN ROAD
 MINNEAPOLIS, MN 55347

Project
TCF BANK

Location
SHOREVIEW, MINNESOTA

Certification
 I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed professional ENGINEER under the laws of the state of Minnesota.

Brad C. Wilkening
 Registration No. 26908 Date: 05/08/2012
 If available, contact us for a wet signed copy of this which is available upon request at MFRA, Inc., Plymouth, MN office.

Summary
 Designed: bcw Drawn: jn
 Approved: MCA Book / Page:
 Phase: PRELIM Initial Issue: 04/30/2012

Revision History
 No. Date By Submittal / Revision
 A 05/08/12 jn CITY COMMENTS

Sheet Title
TITLE SHEET

Sheet No. Revision
C1.01 A

Project No. HTG19272

CONSULTANT CONTACT LIST:

DEVELOPER/OWNER
 TCF BANK
 801 MARQUETTE AVENUE
 MINNEAPOLIS, MN 55402
 612-661-6938
 612-661-8526
 CONTACT: DAVE NELSON

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 PLYMOUTH, MN 55447
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 FAX 763-476-8532
 CONTACT: BRAD C. WILKENING

ARCHITECT
 HTG ARCHITECTS
 9300 HENNEPIN TOWN ROAD
 MINNEAPOLIS, MN 55447
 952-278-8880
 CONTACT: MIKE KRAFT

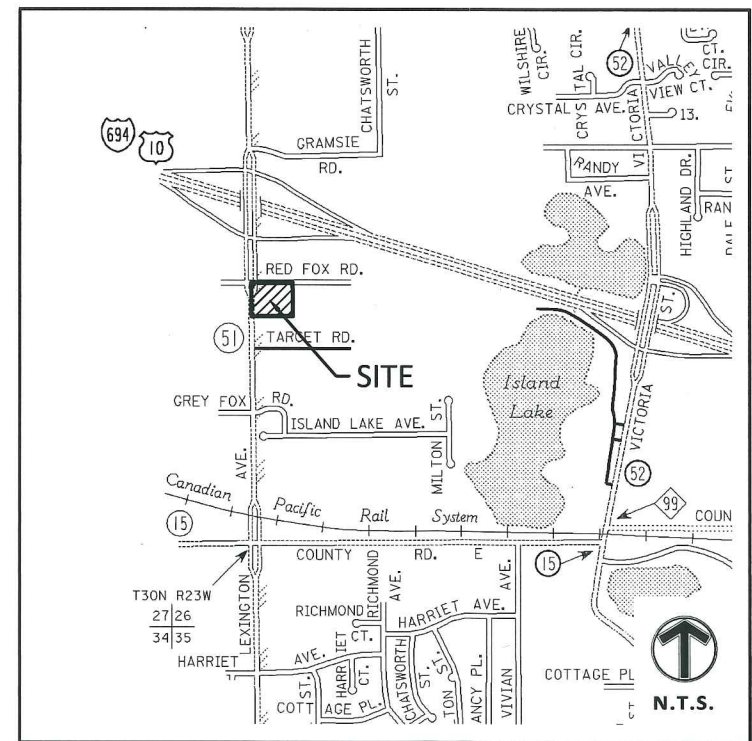
SURVEYOR
 MFRA INC.
 14800 28TH AVENUE, SUITE 140
 PLYMOUTH, MN 55447
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 FAX 763-476-8532
 CONTACT: MARCUS HAMPTON

LANDSCAPING
 MFRA INC.
 14800 28TH AVENUE, SUITE 140
 PLYMOUTH, MN 55447
 TEL 763-476-6010
 FAX 763-476-8532
 CONTACT: KEVIN TEPPEN

BENCHMARKS

BM NO. 1
 TNH AT NE CORNER OF SITE, SOUTH SIDE OF RED FOX ROAD
 ELEV.=948.77

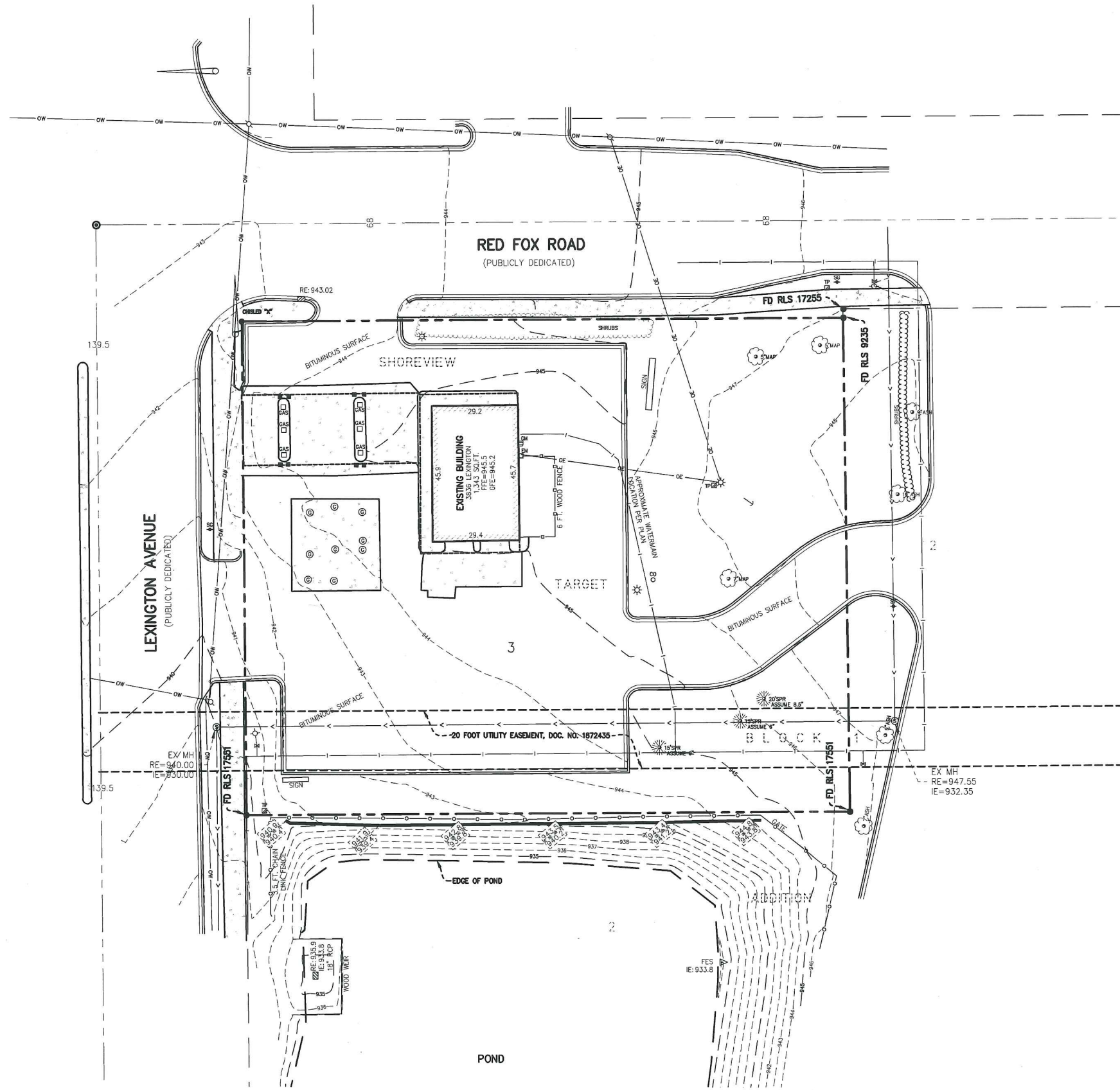
BM NO. 2
 TNH AT SW CORNER OF SITE, EAST SIDE OF LEXINGTON AVENUE
 ELEV.=943.00



VICINITY MAP
 NO SCALE

SHEET INDEX

SHEET	DESCRIPTION
C1.01	TITLE SHEET
C2.01	EXISTING CONDITIONS PLAN
C2.02	DEMOLITION PLAN
C3.01	SITE PLAN
C4.01	GRADING AND DRAINAGE PLAN
C5.01	PHASE I EROSION CONTROL PLAN
C5.02	PHASE II EROSION CONTROL PLAN
C5.03	EROSION CONTROL DETAILS AND NOTES
C6.01	UTILITY PLAN
C9.01	CONSTRUCTION DETAILS
L1.01	LANDSCAPE PLAN
L1.02	LANDSCAPE DETAILS



LEGEND		
● FOUND MONUMENT	—○— WATERMAIN	--- EASEMENT LINE
□ SET MONUMENT	—○— SANITARY SEWER	--- SETBACK LINE
⊙ ELECTRIC METER	—○— STORM SEWER	--- RIGHT OF ACCESS
✱ LIGHT	—○— FLARED END SECTION	--- CONCRETE CURB
⊠ AIR CONDITIONER	⊙ UT ELECTRIC TRANSFORMER	--- BUILDING LINE
— GUY ANCHOR	⊙ UT TELEPHONE PEDESTAL	--- BUILDING CANOPY
⊠ HANDICAP STALL	⊙ UT GAS METER	--- CONCRETE SURFACE
⊠ UTILITY POLE	—○— OVERHEAD WIRE	--- LANDSCAPE SURFACE
■ GUARD POST	—○— CHAIN LINK FENCE	○ DECIDUOUS TREE
● BOLLARD	—○— IRON FENCE	○ CONIFEROUS TREE
➔ SIGN	—○— WIRE FENCE	
	—○— WOOD FENCE	

DESCRIPTION
 LOT 3, BLOCK 1, SHOREVIEW TARGET ADDITION, RAMSEY COUNTY, MINNESOTA

mfra
 engineering surveying planning energy
 14800 28th Ave. N, Ste 140
 Plymouth, Minnesota 55447
 (763) 478.6010 telephone
 (763) 478.8532 facsimile
 www.mfra.com

Client
HTG ARCHITECTS
 9300 HENNEPIN TOWN ROAD
 MINNEAPOLIS, MN 55347

Project
TCF BANK

Location
SHOREVIEW, MINNESOTA

Certification
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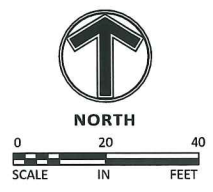
Revision History

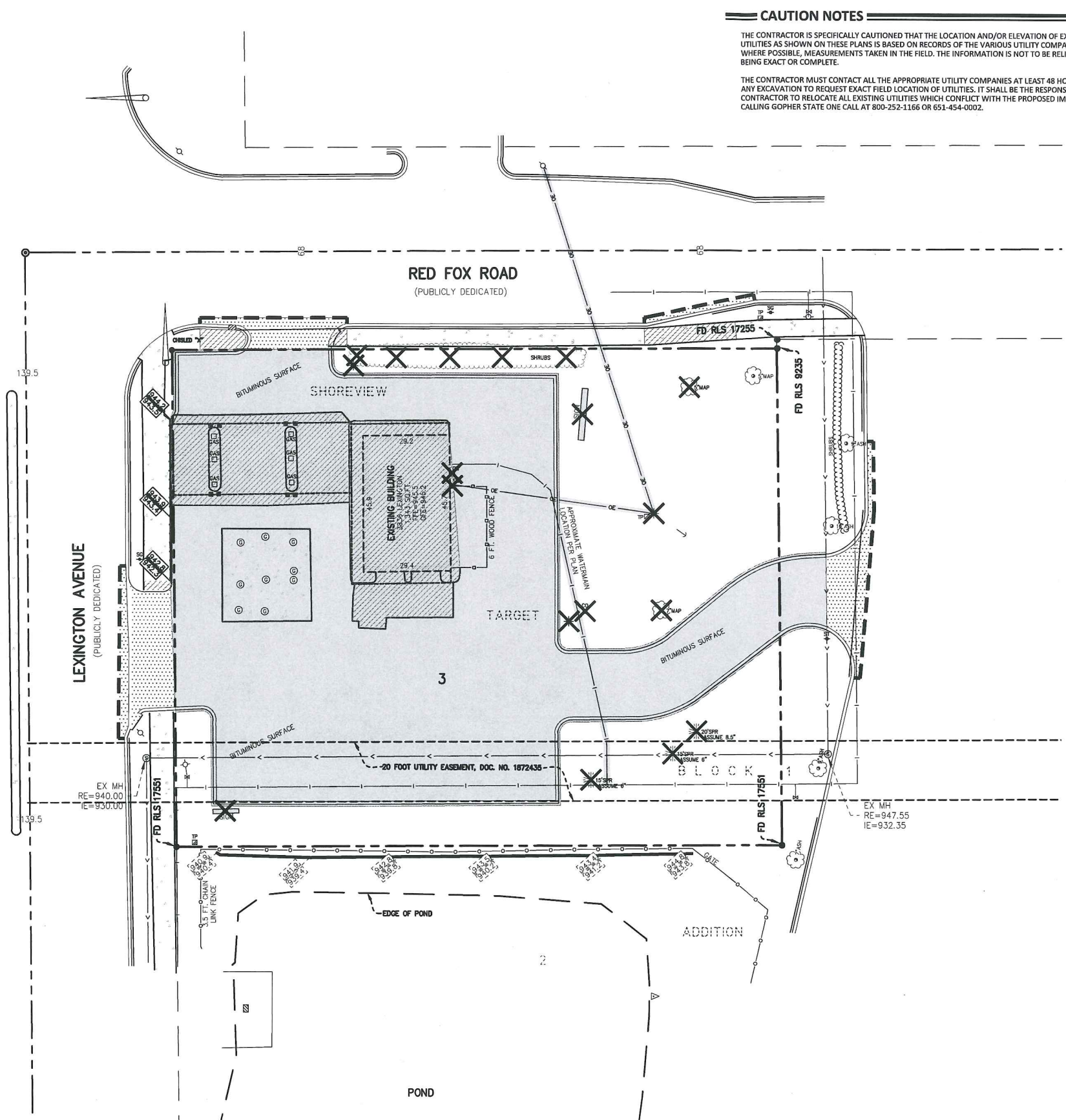
No.	Date By	Submittal / Revision
A	05/08/12 JN	CITY COMMENTS

Sheet Title
EXISTING CONDITIONS PLAN

Sheet No. Revision
C2.01 A

Project No. HTG19272





CAUTION NOTES

THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE.

THE CONTRACTOR MUST CONTACT ALL THE APPROPRIATE UTILITY COMPANIES AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS CALLING GOPHER STATE ONE CALL AT 800-252-1166 OR 651-454-0002.

LEGEND

TO BE REMOVED	EXISTING
CURB & GUTTER	SS
STORM SEWER	SN
SANITARY SEWER	SO
FORCEMAIN (SAN.)	SM
WATERMAIN	WM
EASEMENT	U5
GAS LINE	U6
ELECTRIC	U7
TELEPHONE	U8
BUILDING	
CONCRETE	
WALL	
REMOVE EXISTING	
SAW CUT EXIST BITUMINOUS PAVEMENT	
EXISTING BITUMINOUS TO BE REMOVED	
EXISTING CONCRETE TO BE REMOVED	
EXISTING IMPROVEMENTS TO BE REMOVED BY OTHERS	

GENERAL DEMOLITION NOTES

1. THE CONTRACTOR IS RESPONSIBLE FOR THE DEMOLITION, REMOVAL, AND DISPOSING IN A LOCATION APPROVED BY ALL GOVERNING AUTHORITIES OF ALL DRIVES, UTILITIES, ETC., SUCH THAT THE IMPROVEMENTS SHOWN ON THE REMAINING PLANS CAN BE CONSTRUCTED. BUILDINGS, PADS, TANKS, PUMPING EQUIPMENT, GAS ISLANDS, CONCRETE PADS AND BITUMINOUS WILL BE REMOVED BY OTHERS AS INDICATED.
2. THE CONTRACTOR IS RESPONSIBLE FOR REMOVING ALL DEBRIS FROM THE SITE AND DISPOSING THE DEBRIS IN A LAWFUL MANNER. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED FOR DEMOLITION AND DISPOSAL, AND SHALL PROVIDE COPIES OF ALL PERMITS AND DISPOSAL TICKETS TO THE OWNER AND CITY.
3. THE CONTRACTOR SHALL MAINTAIN ALL UTILITY SERVICES TO THE EXISTING ADJACENT PROPERTIES AT ALL TIMES. UTILITY SERVICES SHALL NOT BE INTERRUPTED WITHOUT NOTIFICATION AND APPROVAL FROM THE CITY OF SHOREVIEW PUBLIC UTILITIES AND THE AFFECTED PROPERTY OWNERS.
4. THE CONTRACTOR SHALL COORDINATE WITH RESPECTIVE UTILITY COMPANIES PRIOR TO THE REMOVAL AND/OR RELOCATION OF UTILITIES. THE CONTRACTOR SHALL COORDINATE WITH THE UTILITY COMPANY CONCERNING PORTIONS OF WORK WHICH MAY BE PERFORMED BY THE UTILITY COMPANY'S FORCES AND ANY FEES WHICH ARE TO BE PAID TO THE UTILITY COMPANY FOR THEIR SERVICES, THE CONTRACTOR IS RESPONSIBLE FOR PAYING ALL FEES AND CHARGES.
5. THE LOCATIONS OF ALL EXISTING UTILITIES SHOWN ON THIS PLAN HAVE BEEN DETERMINED FROM INFORMATION AVAILABLE AND ARE GIVEN FOR THE CONVENIENCE OF THE CONTRACTOR. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THEIR ACCURACY. PRIOR TO THE START OF ANY DEMOLITION ACTIVITY, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES FOR ONSITE LOCATIONS OF EXISTING UTILITIES.
6. ALL EXISTING SEWERS, PIPING AND UTILITIES SHOWN ARE NOT TO BE INTERPRETED AS THE EXACT LOCATION, OR AS THE ONLY OBSTACLES THAT MAY OCCUR ON THE SITE. VERIFY EXISTING CONDITIONS AND PROCEED WITH CAUTION AROUND ANY ANTICIPATED FEATURES. GIVE NOTICE TO ALL UTILITY COMPANIES REGARDING DESTRUCTION AND REMOVAL OF ALL SERVICE LINES AND CAP ALL LINES BEFORE PROCEEDING WITH THE WORK. UTILITIES DETERMINED TO BE ABANDONED AND LEFT IN PLACE SHALL BE GROUTED IF UNDER BUILDING.
7. ALL EXISTING UTILITIES TO BE ABANDONED SHALL BE DISCONNECTED AT THE MAIN LINE. MANHOLES SHALL BE REMOVED OR FILLED IN AN BOTTOMS DRILLED WITH HOLES.
8. ELECTRICAL, TELEPHONE, CABLE, WATER, FIBER OPTIC CABLE AND/OR GAS LINES NEEDING TO BE REMOVED OR RELOCATED SHALL BE COORDINATED WITH THE AFFECTED UTILITY COMPANY. ADEQUATE TIME SHALL BE PROVIDED FOR RELOCATION AND CLOSE COORDINATION WITH THE UTILITY COMPANY IS NECESSARY TO PROVIDE A SMOOTH TRANSITION IN UTILITY SERVICE. CONTRACTOR SHALL PAY CLOSE ATTENTION TO EXISTING UTILITIES WITHIN THE ROAD RIGHT OF WAY DURING CONSTRUCTION.
9. CONTRACTOR MUST PROTECT THE PUBLIC AT ALL TIMES WITH FENCING, BARRICADES, ENCLOSURES, ETC., TO THE BEST PRACTICES AND APPROVED BY CITY OF SHOREVIEW.
10. CONTINUOUS ACCESS SHALL BE MAINTAINED FOR THE SURROUNDING PROPERTIES AT ALL TIMES DURING DEMOLITION OF THE EXISTING FACILITIES.
11. PRIOR TO DEMOLITION OCCURRING, ALL EROSION CONTROL DEVICES ARE TO BE INSTALLED.
12. THE EXISTING BITUMINOUS TO BE REMOVED SHALL BE SAW CUT FULL DEPTH OF THE BITUMINOUS AT THE LIMIT OF THE BITUMINOUS REMOVAL AS SHOWN.
13. CONTRACTOR MAY LIMIT SAW-CUT & PAVEMENT REMOVAL TO ONLY THOSE AREAS WHERE IT IS REQUIRED AS SHOWN ON THESE CONSTRUCTION PLANS BUT IF ANY DAMAGE IS INCURRED ON ANY OF THE SURROUNDING PAVEMENT, ETC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ITS REMOVAL AND REPAIR.
14. THE CONTRACTOR SHALL COORDINATE WATERMAIN WORK WITH THE FIRE DEPARTMENT AND THE CITY UTILITY DEPARTMENT TO PLAN PROPOSED IMPROVEMENTS AND TO ENSURE ADEQUATE FIRE PROTECTION IS CONSTANTLY AVAILABLE TO THE SITE THROUGH OUT THIS SPECIFIC WORK AND THROUGH ALL PHASES OF CONSTRUCTION. CONTRACTOR WILL BE RESPONSIBLE FOR ARRANGING/PROVIDING ANY REQUIRED WATERMAIN SHUT OFFS WITH THE CITY DURING CONSTRUCTION. ANY COSTS ASSOCIATED WITH WATERMAIN SHUT OFFS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR AND NO EXTRA COMPENSATION WILL BE PROVIDED.
15. DAMAGE TO ALL EXISTING ITEMS TO REMAIN WILL BE REPLACED AT CONTRACTOR'S EXPENSE.
16. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS AND REMOVING ALL FUEL TANKS AND EQUIPMENT IN ACCORDANCE TO ALL LOCAL AND MPCA REQUIREMENTS. THE CONTRACTOR SHALL PROVIDE ALL REQUIRED TEST RESULTS AND DOCUMENTATION TO THE OWNER, CITY AND MPCA TO SHOW THE FUEL SYSTEM WAS REMOVED AND DISPOSED OF PROPERLY.

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Project
TCF BANK

Location
SHOREVIEW, MINNESOTA

Certification
 I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed professional ENGINEER under the laws of the state of Minnesota.
 Brad C. Hennings
 Registration No. 26908 Date: 05/08/2012
 If available, contact us for a wet signed copy of this plan, which is available upon request at MFRA, Inc., Plymouth, MN office.

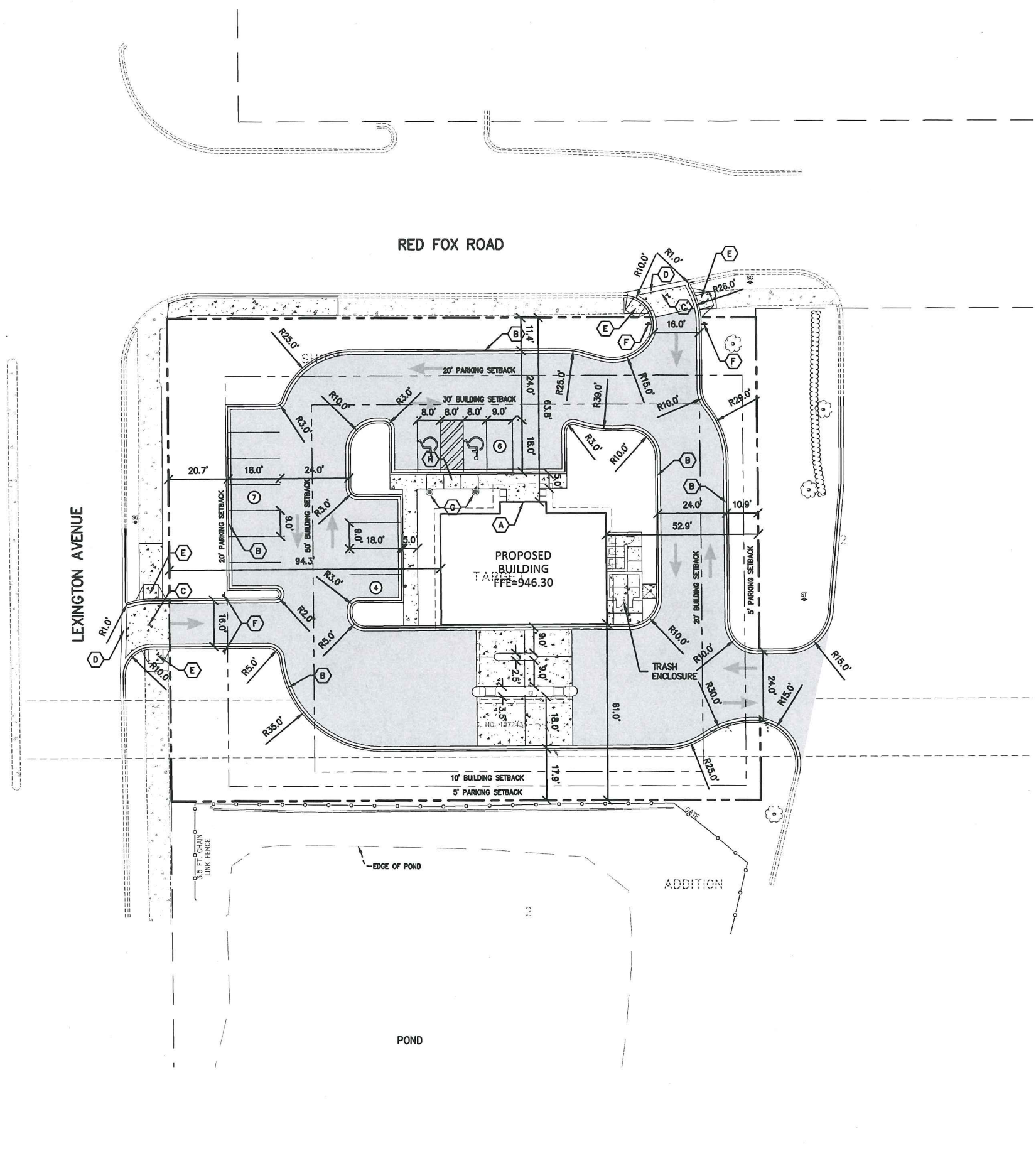
Summary
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 A 05/08/12 JN CITY COMMENTS

Sheet Title
DEMOLITION PLAN

Sheet No. Revision
C2.02 A

Project No. HTG19272



LEGEND

	PROPOSED	EXISTING	
PROPERTY LIMIT	---	---	STANDARD DUTY ASPHALT PAVING
CURB & GUTTER	---	---	CONCRETE PAVING
EASEMENT	---	---	
BUILDING	---	---	
RETAINING WALL	---	---	
SIGN	○	○	
PIPE BOLLARD	●	●	
NUMBER OF PARKING STALLS PER ROW	(X)	(X)	
KEY NOTE	(XX)	(XX)	

DEVELOPMENT SUMMARY

AREA		
GROSS SITE AREA	34,168 SF	0.78 AC
NET SITE AREA	34,168 SF	0.78 AC
BUILDING SETBACKS		
FRONT YARD (OFF LEXINGTON AVENUE)		50 FEET
REAR YARD (EAST)		20 FEET
SIDE YARD (OFF RED FOX ROAD)		30 FEET
SIDE YARD (SOUTH)		10 FEET
PARKING SETBACKS		
FRONT YARD (OFF LEXINGTON AVENUE)		20 FEET
REAR YARD (EAST)		5 FEET
SIDE YARD (OFF RED FOX ROAD)		20 FEET
SIDE YARD (SOUTH)		5 FEET
MINIMUM LOT SIZE		NONE
ZONING		
EXISTING ZONING		PUD
PROPOSED ZONING		CI-PUD
PROPOSED PARKING COUNT		
STANDARD PARKING STALLS		15 STALLS
HANDICAP STALLS		2 STALLS
TOTAL STALLS		17 STALLS

- DEVELOPMENT NOTES**
- ALL DIMENSIONS ARE ROUNDED TO THE NEAREST TENTH FOOT.
 - ALL DIMENSIONS SHOWN ARE TO THE FACE OF CURB TO FACE OF CURB UNLESS OTHERWISE NOTED. BACK OF CURB IS SHOWN GRAPHICALLY ONLY.
 - ALL PARKING STALLS TO BE 9' IN WIDTH AND 18' IN LENGTH UNLESS OTHERWISE INDICATED.
 - CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF EXIT PORCHES, RAMPS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
 - REFER TO FINAL PLAT FOR LOT BOUNDARIES, LOT NUMBERS, LOT AREAS, AND LOT DIMENSIONS.
 - ALL GRADIENTS ON SIDEWALKS ALONG THE ADA ROUTE SHALL HAVE A MAXIMUM LONGITUDINAL SLOPE OF 5% (1:20), EXCEPT AT CURB RAMPS (1:12), AND A MAXIMUM CROSS SLOPE OF 2.08% (1:48). THE MAXIMUM SLOPE IN ANY DIRECTION ON AN ADA PARKING STALL OR ACCESS ISLE SHALL BE 2.08% (1:48). THE CONTRACTOR SHALL REVIEW AND VERIFY THE GRADIENT IN THE FIELD ALONG THE ADA ROUTES PRIOR TO PLACING CONCRETE OR BITUMINOUS. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IF THERE IS A DISCREPANCY BETWEEN THE GRADIENT IN THE FIELD VERSUS THE DESIGN GRADIENT.

- KEY NOTES**
- BUILDING, STOOPS, STAIRS SEE ARCHITECTURAL PLANS.
 - B-612 CONCRETE CURB AND GUTTER.
 - CONCRETE APRON
 - VALLEY GUTTER.
 - ACCESSIBLE RAMP.
 - DO NOT ENTER, ENTRANCE ONLY SIGN.
 - H/C PARKING SIGN WITH BOLLARD.
 - ACCESSIBLE RAMP SPECIAL.

AREA SUMMARY IN ACRES

PAVEMENT AREA	0.47 AC±
BUILDING AREA	0.03 AC±
SEEDING AREA	0.28 AC±
TOTAL DISTURBED	0.78 AC±
PRE - CONSTRUCTION IMPERVIOUS	0.50 AC±
POST - CONSTRUCTION IMPERVIOUS	0.50 AC±

DO NOT ENTER Entrance Only

Handicap Parking Only

NORTH

0 20 40
SCALE IN FEET

F STREET VIEW G FRONT VIEW

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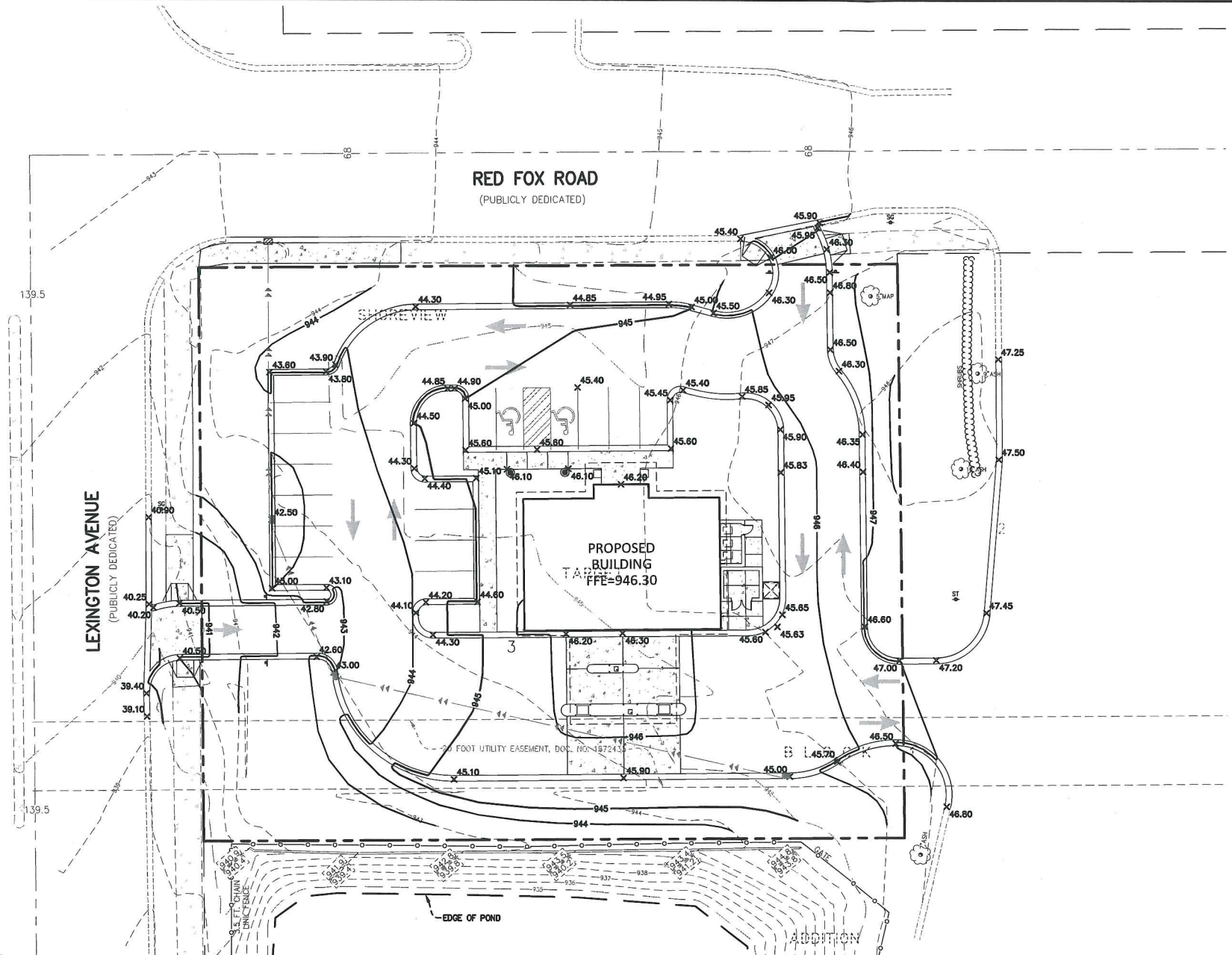
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SITE PLAN

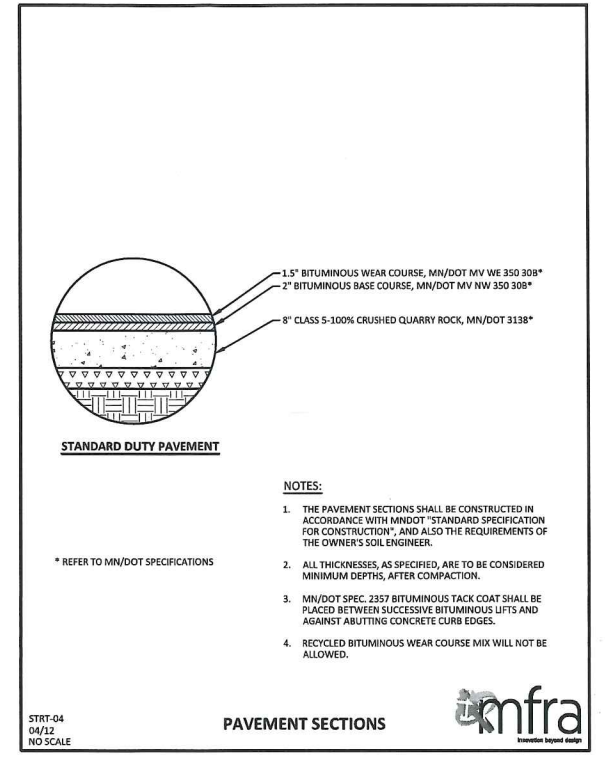
Sheet No. Revision
C3.01 A

Project No. HTG19272



LEGEND

	PROPOSED	EXISTING
PROPERTY LIMIT	---	---
CURB & GUTTER	—●—	—○—
STORM SEWER	—●—	—○—
DRAINILE	—●—	—○—
BUILDING	—	—
RETAINING WALL	—	—
WETLAND LIMITS	—	—
TREELINE	—	—
SPOT ELEVATION	962.5	962.5
CONTOUR	902	902
RIP RAP	EOFF	XXX.X
OVERFLOW ELEV.	902	902
SOIL BORINGS	ST 3	1042.56



GRADING NOTES

- PROPOSED CONTOURS ARE TO FINISHED SURFACE ELEVATION. SPOT ELEVATIONS ALONG PROPOSED CURB DENOTE GUTTER GRADE.
- THE CONTRACTOR IS CAUTIONED THAT "THE SUBSURFACE UTILITY INFORMATION SHOWN ON THESE PLANS IS A UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF C/ASCE 38-02 TITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA". THE CONTRACTOR AND/OR SUBCONTRACTORS SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, BY CONTACTING THE NOTIFICATION CENTER (GOPHER STATE ONE FOR MINNESOTA AT 1-800-252-1166). THE CONTRACTOR AND/OR SUBCONTRACTOR AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY HIS OR HER FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UTILITIES (UNDERGROUND AND OVERHEAD).
IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
- THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT. THE CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR ANY DAMAGES TO THE ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASES OF THIS PROJECT.
- SAFETY NOTICE TO CONTRACTORS: IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS ON THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. THE DUTY OF THE ENGINEER OR THE DEVELOPER TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES IN, ON OR NEAR THE CONSTRUCTION SITE.
- THE CONTRACTOR SHALL COMPLETE THE SITE GRADING CONSTRUCTION IN ACCORDANCE WITH THE REQUIREMENTS OF THE OWNER'S SOILS ENGINEER. ALL SOIL TESTING SHALL BE COMPLETED BY THE OWNER'S SOILS ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOIL TESTS AND INSPECTIONS WITH THE SOILS ENGINEER.
- A GEOTECHNICAL ENGINEERING SOILS REPORT HAS BEEN COMPLETED BY:

COMPANY:	TBD
ADDRESS:	
PHONE:	
DATED:	
- THE CONTRACTOR SHALL OBTAIN A COPY OF THE SOILS REPORT.
- PRIOR TO PLACEMENT OF THE AGGREGATE BASE, A TEST ROLL WILL BE REQUIRED ON THE STREET AND PARKING AREA SUBGRADE. THE CONTRACTOR SHALL PROVIDE A LOADED TANDEM AXLE TRUCK WITH A GROSS WEIGHT OF 25 TONS. THE TEST ROLLING SHALL BE AT THE DIRECTION OF THE SOILS ENGINEER AND SHALL BE COMPLETED IN AREAS AS DIRECTED BY THE SOILS ENGINEER. THE SOILS ENGINEER SHALL DETERMINE WHICH SECTIONS OF THE STREET OR PARKING AREA ARE UNSTABLE. CORRECTION OF THE SUBGRADE SOILS SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SOILS ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. TRAFFIC CONTROL DEVICES SHALL CONFORM TO APPROPRIATE MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARDS.
- THE TREES AND OTHER NATURAL VEGETATION WITHIN THE PROJECT AND/OR ADJACENT TO THE PROJECT ARE OF PRIME CONCERN TO THE CONTRACTOR'S OPERATIONS. HE WILL BE REQUIRED TO PROTECT THE TREES WHICH ARE TO BE SAVED TO BE SURE THAT EQUIPMENT IS NOT NEEDLESSLY OPERATED UNDER NEARBY TREES AND SHALL EXERCISE EXTREME CAUTION IN WORKING ADJACENT TO TREES. SHOULD ANY PORTION OF THE TREE BRANCHES REQUIRE REMOVAL TO PERMIT OPERATION OF THE CONTRACTOR'S EQUIPMENT, HE SHALL OBTAIN THE SERVICES OF A PROFESSIONAL TREE TRIMMING SERVICE TO TRIM THE TREES PRIOR TO THE BEGINNING OF OPERATION. SHOULD THE CONTRACTOR'S OPERATIONS RESULT IN THE BREAKING OF ANY LIMBS, THE BROKEN LIMBS SHOULD BE REMOVED IMMEDIATELY AND CUTS SHALL BE PROPERLY PROTECTED TO MINIMIZE ANY LASTING DAMAGE TO THE TREE. NO TREES SHALL BE REMOVED WITHOUT AUTHORIZATION BY THE ENGINEER. COSTS FOR TRIMMING SERVICES SHALL BE CONSIDERED INCIDENTAL TO THE GRADING CONSTRUCTION AND NO SPECIAL PAYMENT WILL BE MADE.
- RESTRICTED AREAS SHALL INCLUDE ALL DESIGNATED TREADED AREAS OUTSIDE OF THE DESIGNATED CONSTRUCTION ZONE. ALL VEGETATION WITHIN THE RESTRICTED AREAS SHALL REMAIN.
- CONTRACTOR SHALL RESTRICT ALL GRADING AND CONSTRUCTION ACTIVITIES TO AREAS DESIGNATED ON THE PLANS. ACTIVITIES WITHIN THE CONSTRUCTION MAY BE DIRECTED TO A NARROWER WIDTH IN THE FIELD TO SAVE ADDITIONAL TREES AS DIRECTED BY THE OWNER.
- ACTIVITIES PROHIBITED OUTSIDE OF THE CONSTRUCTION BOUNDARIES WOULD INCLUDE, BUT NOT BE LIMITED TO: SOIL AND OTHER MATERIAL STOCKPILING, EQUIPMENT OR MACHINERY STORAGE, DRIVING OF ANY VEHICLE, LEAKAGE OR SPILLAGE OF ANY "WASHOUT" OR OTHER TOXIC MATERIAL. THE COLLECTION OF OTHER DEBRIS AND SOIL STOCKPILING WILL BE IN AN AREA DETERMINED ON-SITE BY THE ENGINEER.
- ALL RESTRICTED AREAS SHALL BE FENCED OFF WITH BRIGHT ORANGE POLYETHYLENE SAFETY NETTING AND STEEL STAKES AS SHOWN ON THE TREE PROTECTION DETAIL. AT NO TIME SHALL THIS FENCING BE REMOVED OR ACTIVITY OF ANY KIND TAKE PLACE WITHIN IT. FINAL PLACEMENT OF ALL PROTECTIVE FENCING SHALL BE COMPLETE BEFORE ANY WORK COMMENCES ON-SITE.
- BEFORE COMMENCING WITH ANY EXCAVATION THE CONTRACTOR SHALL COMPLETE ALL PREPARATORY WORK REGARDING TREE REMOVAL, ROOT PRUNING, TREE PRUNING AND STUMP REMOVAL TO THE SATISFACTION OF THE OWNER.
- PREPARATORY WORK SHALL INCLUDE THE FOLLOWING AND SHALL BE COMPLETED UNDER THE DIRECT SUPERVISION OF THE OWNER'S REPRESENTATIVE:
 - TREE REMOVAL: THE CONTRACTOR SHALL FELL THE TREES. AT NO TIME SHALL TREES BE BULLDOZED OUT, BUT SHALL BE CUT DOWN AND STUMPS REMOVED SEPARATELY. PRIOR TO THE FELLING OF ALL TREES, PROPER REMOVAL OF A PORTION OR ALL OF THE CANOPY SHALL BE COMPLETED SO THAT TREES IN THE RESTRICTED AREAS SHALL NOT BE INJURED IN THE PROCESS.
 - TREE PRUNING: PROPER PRUNING OF TREES IN THE RESTRICTED ZONE SHALL BE DIRECTED BY AND SUPERVISION AT ALL TIMES BY THE OWNER'S REPRESENTATIVE.
 - EXCAVATE TOPSOIL FROM AREAS TO BE FURTHER EXCAVATED OR REGRADED AND STOCKPILE IN AREAS DESIGNATED ON THE SITE. THE CONTRACTOR SHALL SALVAGE ENOUGH TOPSOIL FOR RESPREADING ON THE SITE AS SPECIFIED. EXCESS TOPSOIL SHALL BE PLACED IN EMBANKMENT AREAS, OUTSIDE OF BUILDING PADS, ROADWAYS AND PARKING AREAS. THE CONTRACTOR SHALL SUBCUT CUT AREAS, WHERE TURF IS TO BE ESTABLISHED, TO A DEPTH OF 4 INCHES. RESPREAD TOPSOIL IN AREAS WHERE TURF IS TO BE ESTABLISHED TO A MINIMUM DEPTH OF 4 INCHES.
- TOLERANCES
 - THE BUILDING SUBGRADE FINISHED SURFACE ELEVATION SHALL NOT VARY BY MORE THAN 0.10 FOOT ABOVE, OR 0.10 FOOT BELOW, THE PRESCRIBED ELEVATION AT ANY POINT WHERE MEASUREMENT IS MADE.
 - THE STREET OR PARKING AREA SUBGRADE FINISHED SURFACE ELEVATION SHALL NOT VARY BY MORE THAN 0.05 FOOT ABOVE, OR 0.10 FOOT BELOW, THE PRESCRIBED ELEVATION OF ANY POINT WHERE MEASUREMENT IS MADE.
 - AREAS WHICH ARE TO RECEIVE TOPSOIL SHALL BE GRADED TO WITHIN 0.30 FOOT ABOVE OR BELOW THE REQUIRED ELEVATION, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
 - TOPSOIL SHALL BE GRADED TO PLUS OR MINUS 1/2 INCH OF THE SPECIFIED THICKNESS.
 - AFTER THE SITE GRADING IS COMPLETED, IF EXCESS OR SHORTAGE OF SOIL MATERIAL EXISTS, THE CONTRACTOR SHALL TRANSPORT ALL EXCESS SOIL MATERIAL OFF THE SITE TO AN AREA SELECTED BY THE CONTRACTOR, OR IMPORT SUITABLE MATERIAL TO THE SITE.
- WHEN THE SITE GRADING CONSTRUCTION IS COMPLETED, THE CONTRACTOR SHALL SEED AND MULCH THOSE AREAS AS DIRECTED BY THE OWNER. THE SEEDING SHALL COMPLY WITH MN/DOT SPECIFICATION NO. 3876 WITH THE USE OF MN/DOT MIXTURE NO. 50B OR 250 GR AND APPLIED AT A RATE OF 100 POUNDS PER ACRE. THE SEEDED AREA SHALL BE MULCHED. THE MULCH SHALL COMPLY WITH MN/DOT SPECIFICATION 3882, TYPE 1. THE MULCH SHALL BE APPLIED AND ANCHORED IN ACCORDANCE WITH MN/DOT SPECIFICATION NO. 2575-3, F1. MULCH SHALL BE APPLIED AT THE RATE OF 2 TONS PER ACRE. THE MULCH SHALL BE ANCHORED WITH A DISC, CLODBUSTER OR OTHER APPROVED EQUIPMENT.
- THE CONTRACTOR SHALL DETERMINE THE LOCATION OF ANY HAUL ROADS THAT MAY BE REQUIRED TO COMPLETE THE SITE GRADING CONSTRUCTION AND SHALL INDICATE HAUL ROADS ON EROSION AND SEDIMENT CONTROL "SITE MAP". THE CONTRACTOR SHALL COMPLY WITH THE REQUIREMENTS OF THE GOVERNING AUTHORITY OF EACH ROADWAY. THE CONTRACTOR SHALL POST WHATEVER SECURITY, AND COMPLY WITH ALL CONDITIONS WHICH ARE REQUIRED BY EACH GOVERNING AUTHORITY OF EACH ROADWAY.

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Sheet Title
GRADING AND DRAINAGE PLAN

Sheet No. Revision
C4.01 A

Project No. HTG19272

NOTE TO CONTRACTOR

THE EROSION CONTROL PLAN SHEETS ALONG WITH THE REST OF THE SWPPP MUST BE KEPT ONSITE UNTIL THE NOTICE OF TERMINATION IS FILED WITH THE MPCA. THE CONTRACTOR MUST UPDATE THE SWPPP, INCLUDING THE EROSION CONTROL PLAN SHEETS AS NECESSARY TO INCLUDE ADDITIONAL REQUIREMENTS, SUCH AS ADDITIONAL OR MODIFIED BMPs DESIGNED TO CORRECT PROBLEMS IDENTIFIED. AFTER FILING THE NOTICE OF TERMINATION, THE SWPPP, INCLUDING THE EROSION CONTROL PLAN SHEETS, AND ALL REVISIONS TO IT MUST BE SUBMITTED TO THE OWNER, TO BE KEPT ON FILE IN ACCORDANCE WITH THE RECORD RETENTION REQUIREMENTS DESCRIBED IN THE SWPPP NARRATIVE.

LEGEND

- PROPOSED
- EXISTING
- CURB & GUTTER
- STORM SEWER
- DRAIN TILE
- CONTOUR
- RIP RAP
- OVERFLOW ELEV.
- SILT FENCE
- SILT DIKE
- LIMITS OF DISTURBANCE
- SOIL BORINGS
- DIRECTION OF OVERLAND FLOW
- TEMPORARY DIVERSION
- DITCH
- CHECK DAM
- LIMITS OF DRAINAGE SUB-BASIN
- BIO-ROLL
- INLET PROTECTION DEVICE 1
- TEMPORARY STONE CONSTRUCTION ENTRANCE
- TEMPORARY SEDIMENT BASIN
- TEMPORARY STORAGE AND PARKING AREA
- TEMPORARY STABILIZATION MEASURES (SEED, MULCH, MATS OR BLANKETS AS OUTLINED IN THE SWPPP)

SEQUENCE OF CONSTRUCTION

- PHASE I:**
- INSTALL STABILIZED CONSTRUCTION ENTRANCES.
 - PREPARE TEMPORARY PARKING AND STORAGE AREA.
 - CONSTRUCT THE SILT FENCES ON THE SITE.
 - CONSTRUCT THE SEDIMENTATION AND SEDIMENT TRAP BASINS.
 - HALT ALL ACTIVITIES AND CONTACT THE CIVIL ENGINEERING CONSULTANT TO PERFORM INSPECTION OF BMPs. GENERAL CONTRACTOR SHALL SCHEDULE AND CONDUCT STORM WATER PRE-CONSTRUCTION MEETING WITH ENGINEER AND ALL GROUND DISTURBING CONTRACTORS BEFORE PROCEEDING WITH CONSTRUCTION.
 - CLEAR AND GRUB THE SITE.
 - BEGIN GRADING THE SITE.
 - START CONSTRUCTION OF BUILDING PAD AND STRUCTURES.
- PHASE II:**
- TEMPORARILY SEED DENUDED AREAS.
 - INSTALL UTILITIES, UNDERDRAINS, STORM SEWERS, CURBS AND GUTTERS.
 - INSTALL RIP RAP AROUND OUTLET STRUCTURES.
 - INSTALL INLET PROTECTION AROUND ALL STORM SEWER STRUCTURES.
 - PREPARE SITE FOR PAVING.
 - PAVE SITE.
 - INSTALL INLET PROTECTION DEVICES.
 - COMPLETE GRADING AND INSTALL PERMANENT SEEDING AND PLANTING.
 - REMOVE ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES (ONLY IF SITE IS STABILIZED), IF REQUIRED BY THE CONTRACT.

EROSION CONTROL MATERIALS QUANTITIES

ITEM	UNIT	QUANTITY
SILT FENCE	LINEAR FEET	700
CONSTRUCTION ENTRANCE	UNIT	1
INLET PROTECTION DEVICE (IP-1)	UNIT	1

AREA SUMMARY IN ACRES

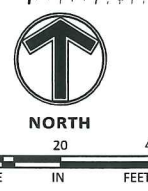
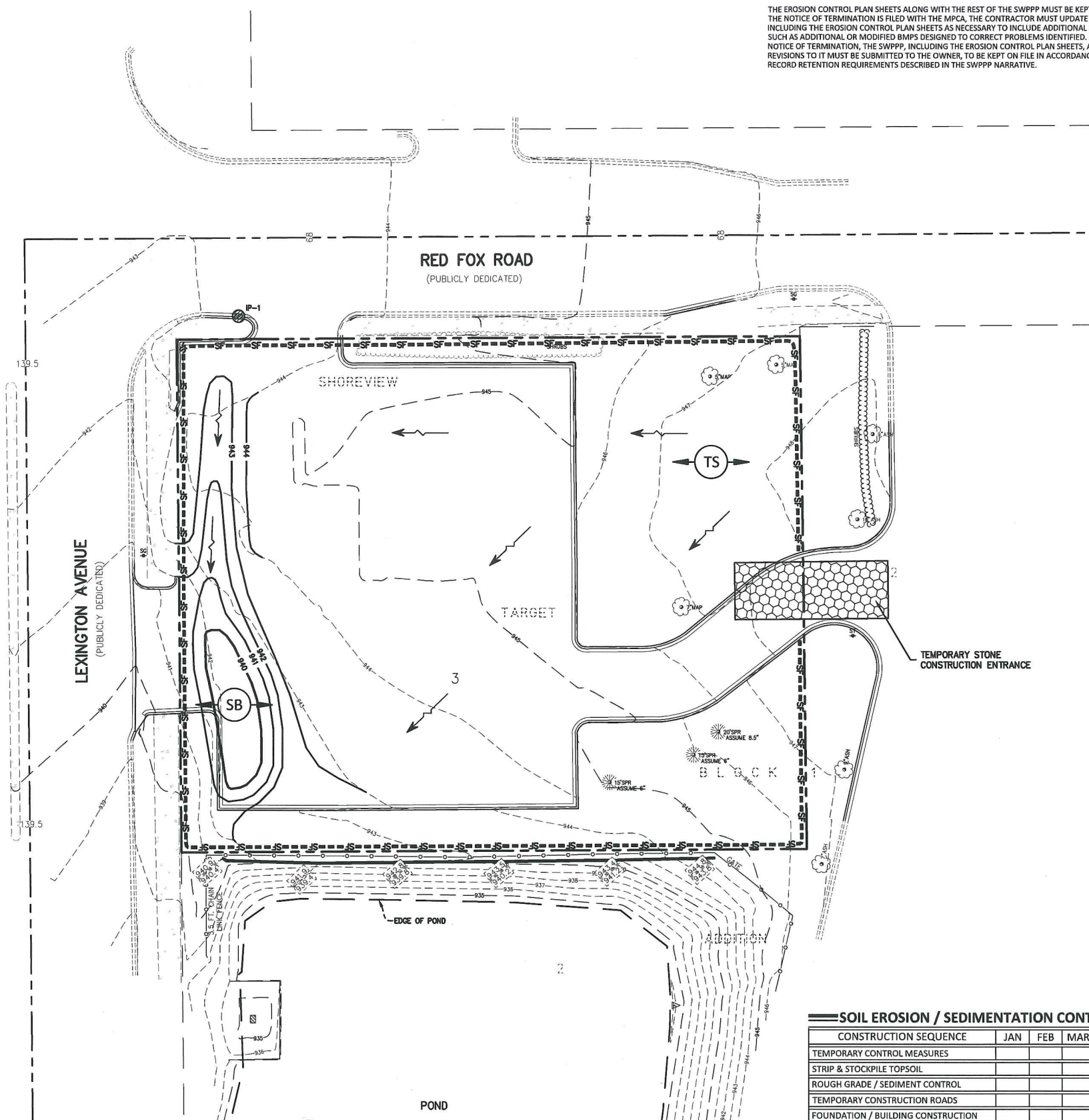
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POST - CONSTRUCTION IMPERVIOUS	0.50 AC±

* REFER TO SHEET C5.03 FOR GENERAL NOTES, MAINTENANCE NOTES, LOCATION MAPS, AND STANDARD DETAILS

SOIL EROSION / SEDIMENTATION CONTROL OPERATION TIME SCHEDULE

CONSTRUCTION SEQUENCE	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC	JAN	FEB	MAR	APR	MAY	JUN	
TEMPORARY CONTROL MEASURES																			
STRIP & STOCKPILE TOPSOIL																			
ROUGH GRADE / SEDIMENT CONTROL																			
TEMPORARY CONSTRUCTION ROADS																			
FOUNDATION / BUILDING CONSTRUCTION																			
SITE CONSTRUCTION																			
PERMANENT CONTROL STRUCTURES																			
FINISH GRADING																			
LANDSCAPING / SEED / FINAL STABILIZATION																			
STORM FACILITIES																			

NOTE: CONTRACTOR OR GENERAL CONTRACTOR TO COMPLETE TABLE WITH THEIR SPECIFIC PROJECT SCHEDULE



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 9300 HENNEPIN TOWN ROAD
 MINNEAPOLIS, MN 55347

Project
TCF BANK

Location
SHOREVIEW, MINNESOTA

Certification
 I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed professional ENGINEER under the laws of the state of Minnesota.
 Brad C. ...
 Registration No. 26908 Date: 05/08/2012
 If available, contact us for a web signed copy of this which is available upon request at MFRA, Inc., Plymouth, MN office.

Summary
 Designed: bcw Drawn: JN
 Approved: MCA Book / Page:
 Phase: PRELIM Initial Issue: 04/30/2012

Revision History
 No. Date By Submittal / Revision
 A 05/08/12 JN CITY COMMENTS

Sheet Title
PHASE I EROSION CONTROL PLAN

Sheet No. Revision
C5.01 A

Project No. HTG19272

NOTE TO CONTRACTOR

THE EROSION CONTROL PLAN SHEETS ALONG WITH THE REST OF THE SWPPP MUST BE KEPT ONSITE UNTIL THE NOTICE OF TERMINATION IS FILED WITH THE MPCA. THE CONTRACTOR MUST UPDATE THE SWPPP, INCLUDING THE EROSION CONTROL PLAN SHEETS AS NECESSARY TO INCLUDE ADDITIONAL REQUIREMENTS, SUCH AS ADDITIONAL OR MODIFIED BMPs DESIGNED TO CORRECT PROBLEMS IDENTIFIED. AFTER FILING THE NOTICE OF TERMINATION, THE SWPPP, INCLUDING THE EROSION CONTROL PLAN SHEETS, AND ALL REVISIONS TO IT MUST BE SUBMITTED TO THE OWNER, TO BE KEPT ON FILE IN ACCORDANCE WITH THE RECORD RETENTION REQUIREMENTS DESCRIBED IN THE SWPPP NARRATIVE.

LEGEND

CURB & GUTTER	PROPOSED	EXISTING
STORM SEWER		
DRAINTILE		
CONTOUR		
RIP RAP		
OVERFLOW ELEV.		
SILT FENCE		
SILT DIKE		
LIMITS OF DISTURBANCE		
SOIL BORINGS		ST-9 PA-10
DIRECTION OF OVERLAND FLOW		
TEMPORARY DIVERSION DITCH		
CHECK DAM		
LIMITS OF DRAINAGE SUB-BASIN		
BIO-ROLL		
INLET PROTECTION DEVICE 1		
TEMPORARY STONE CONSTRUCTION ENTRANCE		
TEMPORARY SEDIMENT BASIN		SB
TEMPORARY STORAGE AND PARKING AREA		TS
TEMPORARY STABILIZATION MEASURES (SEED, MULCH, MATS OR BLANKETS AS OUTLINED IN THE SWPPP)		TSM

- SEQUENCE OF CONSTRUCTION**
- PHASE I:**
1. INSTALL STABILIZED CONSTRUCTION ENTRANCES.
 2. PREPARE TEMPORARY PARKING AND STORAGE AREA.
 3. CONSTRUCT THE SILT FENCES ON THE SITE.
 4. CONSTRUCT THE SEDIMENTATION AND SEDIMENT TRAP BASINS.
 5. HALT ALL ACTIVITIES AND CONTACT THE CIVIL ENGINEERING CONSULTANT TO PERFORM INSPECTION OF BMPs. GENERAL CONTRACTOR SHALL SCHEDULE AND CONDUCT STORM WATER PRE-CONSTRUCTION MEETING WITH ENGINEER AND ALL GROUND DISTURBING CONTRACTORS BEFORE PROCEEDING WITH CONSTRUCTION.
 6. CLEAR AND GRUB THE SITE.
 7. BEGIN GRADING THE SITE.
 8. START CONSTRUCTION OF BUILDING PAD AND STRUCTURES.
- PHASE II:**
1. TEMPORARILY SEED DENUDED AREAS.
 2. INSTALL UTILITIES, UNDERDRAINS, STORM SEWERS, CURBS AND GUTTERS.
 3. INSTALL RIP RAP AROUND OUTLET STRUCTURES.
 4. INSTALL INLET PROTECTION AROUND ALL STORM SEWER STRUCTURES.
 5. PREPARE SITE FOR PAVING.
 6. PAVE SITE.
 7. INSTALL INLET PROTECTION DEVICES.
 8. COMPLETE GRADING AND INSTALL PERMANENT SEEDING AND PLANTING.
 9. REMOVE ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES (ONLY IF SITE IS STABILIZED), IF REQUIRED BY THE CONTRACT.

EROSION CONTROL MATERIALS QUANTITIES

ITEM	UNIT	QUANTITY
SILT FENCE	LINEAR FEET	510
INLET PROTECTION DEVICE (IP-1)	UNIT	4

AREA SUMMARY IN ACRES

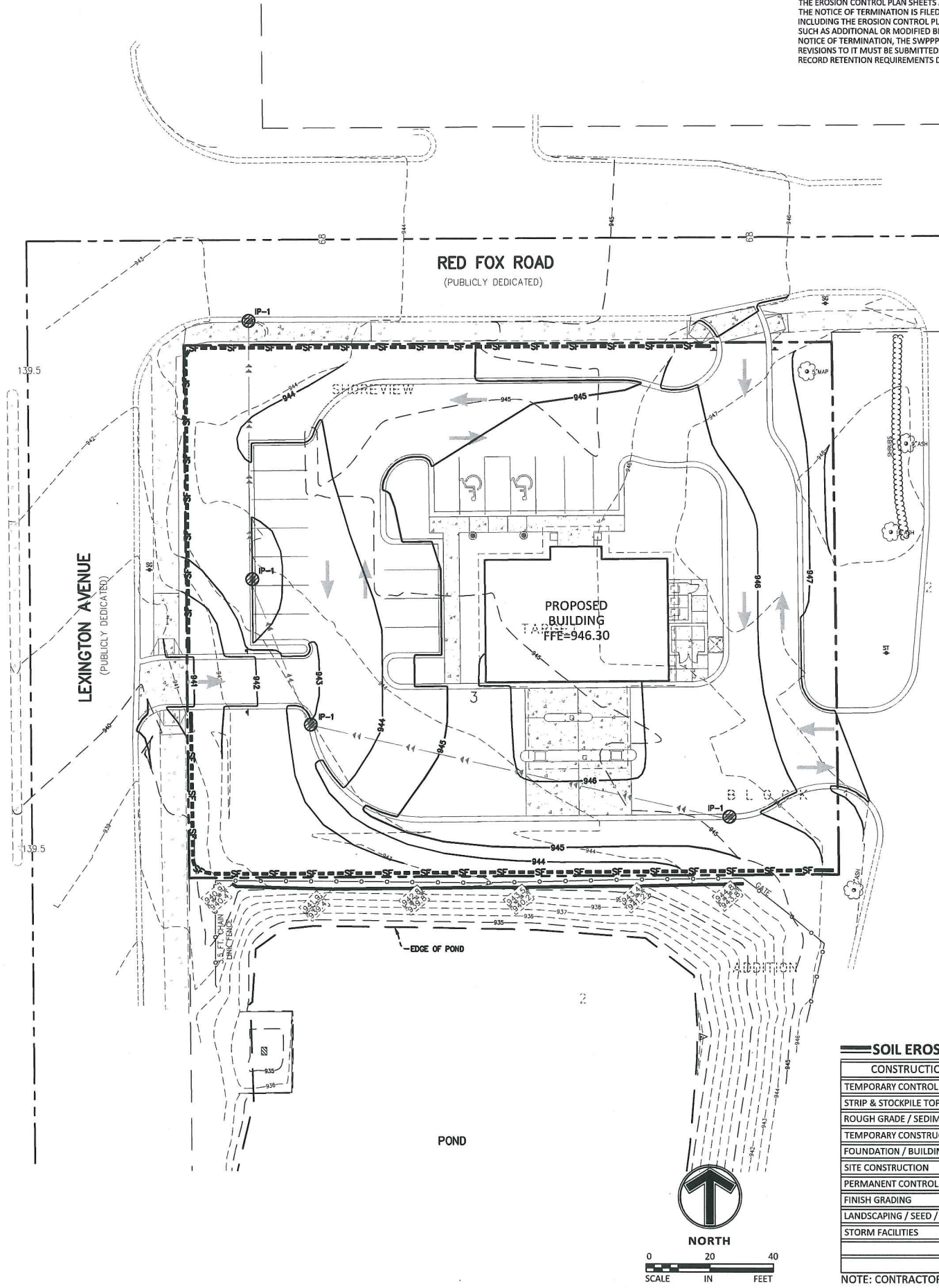
PAVEMENT AREA	0.44 AC±
BUILDING AREA	0.05 AC±
SEEDED AND LANDSCAPE AREA	0.29 AC±
TOTAL DISTURBED	0.78 AC±
PRE - CONSTRUCTION IMPERVIOUS	0.50 AC±
POST - CONSTRUCTION IMPERVIOUS	0.49 AC±

* REFER TO SHEET C5.03 FOR GENERAL NOTES, MAINTENANCE NOTES, LOCATION MAPS, AND STANDARD DETAILS

SOIL EROSION / SEDIMENTATION CONTROL OPERATION TIME SCHEDULE

CONSTRUCTION SEQUENCE	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC	JAN	FEB	MAR	APR	MAY	JUN	
TEMPORARY CONTROL MEASURES																			
STRIP & STOCKPILE TOPSOIL																			
ROUGH GRADE / SEDIMENT CONTROL																			
TEMPORARY CONSTRUCTION ROADS																			
FOUNDATION / BUILDING CONSTRUCTION																			
SITE CONSTRUCTION																			
PERMANENT CONTROL STRUCTURES																			
FINISH GRADING																			
LANDSCAPING / SEED / FINAL STABILIZATION																			
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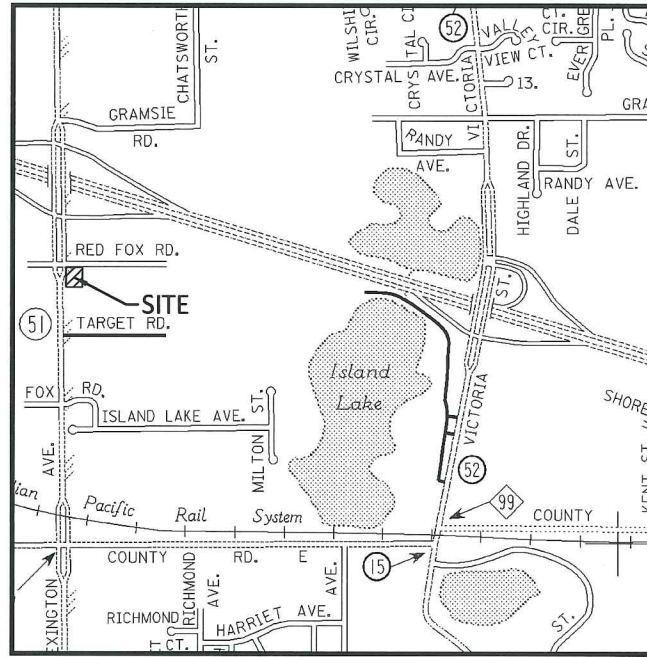
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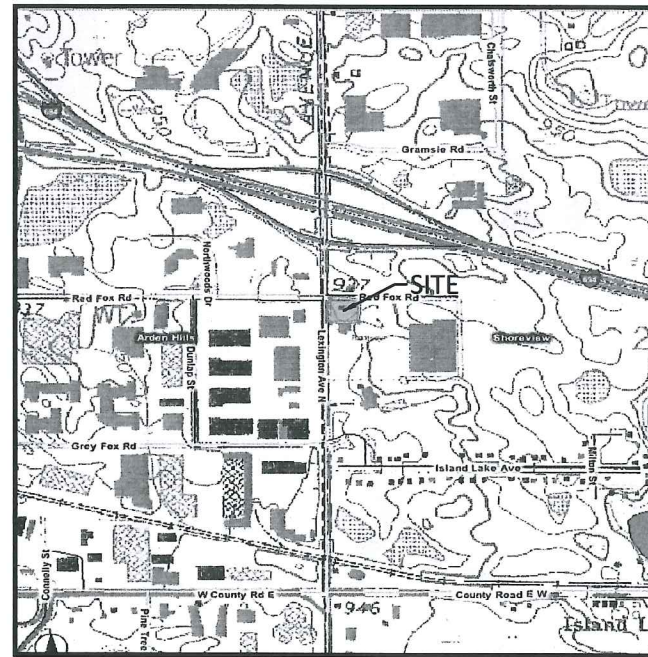
Sheet Title
PHASE II EROSION CONTROL PLAN

Sheet No. Revision
C5.02 A

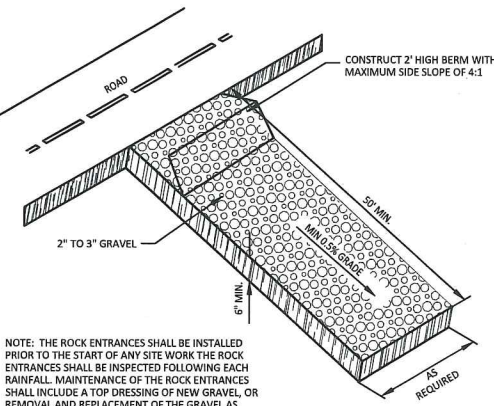
Project No. HTG19272



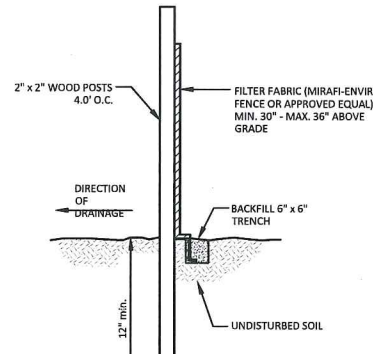
SITE LOCATION MAP



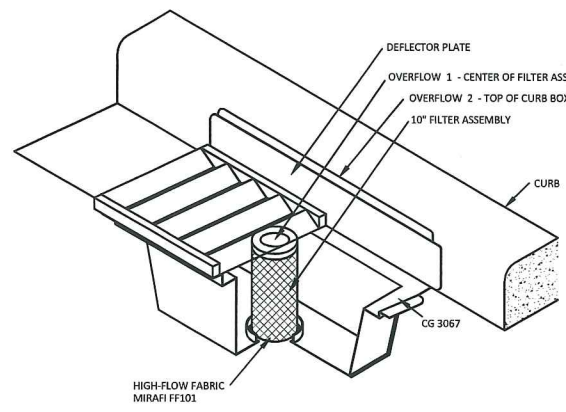
USGS MAP



ROCK ENTRANCE DRIVE
NOT TO SCALE



SILT FENCE DETAIL
NOT TO SCALE



ROAD DRAIN INLET PROTECTION (IP-1)
NOT TO SCALE

SEQUENCE OF CONSTRUCTION

- PHASE I:**
1. INSTALL STABILIZED CONSTRUCTION ENTRANCES.
 2. PREPARE TEMPORARY PARKING AND STORAGE AREA.
 3. CONSTRUCT THE SILT FENCES ON THE SITE.
 4. CONSTRUCT THE SEDIMENTATION AND SEDIMENT TRAP BASINS.
 5. HALT ALL ACTIVITIES AND CONTACT THE CIVIL ENGINEERING CONSULTANT TO PERFORM INSPECTION OF BMP'S. GENERAL CONTRACTOR SHALL SCHEDULE AND CONDUCT STORM WATER PRE-CONSTRUCTION MEETING WITH ENGINEER AND ALL GROUND DISTURBING CONTRACTORS BEFORE PROCEEDING WITH CONSTRUCTION.
 6. CLEAR AND GRUB THE SITE.
 7. BEGIN GRADING THE SITE.
 8. START CONSTRUCTION OF BUILDING PAD AND STRUCTURES.
- PHASE II:**
1. TEMPORARILY SEED DENUDED AREAS.
 2. INSTALL UTILITIES, UNDERDRAINS, STORM SEWERS, CURBS AND GUTTERS.
 3. INSTALL RIP RAP AROUND OUTLET STRUCTURES.
 4. INSTALL INLET PROTECTION AROUND ALL STORM SEWER STRUCTURES.
 5. PREPARE SITE FOR PAVING.
 6. PAVE SITE.
 7. INSTALL INLET PROTECTION DEVICES.
 8. COMPLETE GRADING AND INSTALL PERMANENT SEEDING AND PLANTING.
 9. REMOVE ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES (ONLY IF SITE IS STABILIZED), IF REQUIRED BY THE CONTRACT

ACREAGE SUMMARY IN ACRES	
PAVEMENT AREA	0.44 AC±
BUILDING AREA	0.05 AC±
SEEDING AREA	0.29 AC±
TOTAL DISTURBED	0.78 AC±

DEVELOPER/OWNER:	
SITE OPERATOR / CONTRACTOR/GENERAL CONTRACTOR	
SUPERINTENDENT:	

GENERAL EROSION CONTROL NOTES

1. CONSTRUCTION SHALL COMPLY WITH ALL APPLICABLE GOVERNING CODES AND BE CONSTRUCTED TO SAME. WHERE A CONFLICT EXISTS BETWEEN LOCAL JURISDICTIONAL STANDARD SPECIFICATIONS AND MFRA STANDARD SPECIFICATIONS, THE MORE STRINGENT SPECIFICATION SHALL APPLY.
 2. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATIONS AND/OR ELEVATIONS OF EXISTING UTILITIES AS SHOWN ON THESE PLANS ARE BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE SUBSURFACE UTILITY INFORMATION SHOWN ON THESE PLANS IS A UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-02, ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA." THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CONTACT ALL THE APPROPRIATE UTILITY COMPANIES AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS. THE LOCATIONS OF SMALL UTILITIES SHALL BE OBTAINED BY THE CONTRACTOR BY CALLING MINNESOTA GOPHER STATE ONE CALL AT 800-252-1166 OR 651-454-0002.
 3. THE DESIGN SHOWN IS BASED UPON THE ENGINEER'S UNDERSTANDING OF THE EXISTING CONDITIONS. THE EXISTING CONDITIONS SHOWN ON THIS PLAN ARE BASED UPON AN ALTA OR TOPOGRAPHIC SURVEY PREPARED BY MFRA DATED 03-17-2008. IF CONTRACTOR DOES NOT ACCEPT EXISTING TOPOGRAPHY AS SHOWN ON THE PLANS WITHOUT EXCEPTION, THEY SHALL HAVE MADE, AT THEIR EXPENSE, A TOPOGRAPHIC SURVEY BY A REGISTERED LAND SURVEYOR AND SUBMIT IT TO THE OWNER FOR REVIEW. SEE ATTACHED SURVEY SHEETS.
 4. THE GENERAL CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT. THE CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR ANY DAMAGES OCCURRING TO THE ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT.
 5. THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP) IS COMPRISED OF THIS DRAWING (EROSION & SEDIMENTATION CONTROL PLAN-ESC PLAN), THE STANDARD DETAILS, THE PLAN NARRATIVE, AND ITS APPENDICES, PLUS THE PERMIT AND ALL SUBSEQUENT REPORTS AND RELATED DOCUMENTS.
 6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETING & SUBMITTING THE APPLICATION FOR THE MFRA GENERAL STORMWATER PERMIT FOR CONSTRUCTION ACTIVITY. ALL CONTRACTORS AND SUBCONTRACTORS INVOLVED WITH STORM WATER POLLUTION PREVENTION SHALL OBTAIN A COPY OF THE SWPPP AND THE STATE OF MINNESOTA NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM GENERAL PERMIT (NPDES PERMIT) AND BECOME FAMILIAR WITH THEIR CONTENTS. THE SWPPP AND ALL OTHER RELATED DOCUMENTS MUST BE KEPT AT THE SITE DURING CONSTRUCTION.
 7. CONTRACTOR SHALL IMPLEMENT BEST MANAGEMENT PRACTICES (BMP'S) AS REQUIRED BY THE SWPPP & PERMITS. THE CONTRACTOR SHALL OVERSEE THE INSPECTION & MAINTENANCE OF THE BMP'S AND EROSION PREVENTION FROM BEGINNING OF CONSTRUCTION AND UNTIL CONSTRUCTION IS COMPLETED, IS APPROVED BY ALL AUTHORITIES, THE NOTICE OF TERMINATION (NOT) HAS BEEN FILED WITH THE MFRA BY EITHER THE OWNER OR OPERATOR AS APPROVED ON PERMIT. ADDITIONAL BMP'S SHALL BE IMPLEMENTED AS DICTATED BY CONDITIONS AT NO ADDITIONAL COST TO OWNER THROUGHOUT ALL PHASES OF CONSTRUCTION.
 8. BMP'S AND CONTROLS SHALL CONFORM TO FEDERAL, STATE, OR LOCAL REQUIREMENTS OR MANUAL OF PRACTICE, AS APPLICABLE. CONTRACTOR SHALL IMPLEMENT ADDITIONAL CONTROLS AS DIRECTED BY PERMITTING AGENCY OR OWNER.
 9. ESC PLAN MUST CLEARLY DELINEATE ALL STATE WATERS. PERMITS FOR ANY CONSTRUCTION ACTIVITY IMPACTING STATE WATERS OR REGULATED WETLANDS MUST BE MAINTAINED ON SITE AT ALL TIMES.
 10. CONTRACTOR SHALL MINIMIZE CLEARING TO THE MAXIMUM EXTENT PRACTICAL OR AS REQUIRED BY THE GENERAL PERMIT. THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THE ESC PLAN SHALL BE CLEARLY DELINEATED (E.G. WITH FLAGS, STAKES, SIGNS, SILT FENCE, ETC.) ON THE DEVELOPMENT SITE BEFORE WORK BEGINS. GROUND DISTURBING ACTIVITIES MUST NOT OCCUR OUTSIDE THE LIMITS OF DISTURBANCE.
 11. GENERAL CONTRACTOR SHALL DENOTE ON PLAN THE TEMPORARY PARKING AND STORAGE AREA WHICH SHALL ALSO BE USED AS THE EQUIPMENT MAINTENANCE AND CLEANING AREA, EMPLOYEE PARKING AREA, AND AREA FOR LOCATING PORTABLE FACILITIES, OFFICE TRAILERS, AND TOILET FACILITIES.
 12. ALL WASH WATER (CONCRETE TRUCKS, VEHICLE CLEANING, EQUIPMENT CLEANING, ETC.) MUST BE LIMITED TO A DEFINED AREA OF THE SITE AND SHALL BE CONTAINED AND PROPERLY TREATED OR DISPOSED. NO ENGINE DEGREASING IS ALLOWED ON SITE.
 13. SUFFICIENT OIL AND GREASE ABSORBING MATERIALS AND FLOTATION BOOMS SHALL BE MAINTAINED ON SITE OR READILY AVAILABLE TO CONTAIN AND CLEAN-UP FUEL OR CHEMICAL SPILLS AND LEAKS.
 14. DUST ON THE SITE SHALL BE CONTROLLED. THE USE OF MOTOR OILS AND OTHER PETROLEUM BASED OR TOXIC LIQUIDS FOR DUST SUPPRESSION OPERATIONS IS PROHIBITED.
 15. SOLID WASTE: COLLECTED SEDIMENT, ASPHALT & CONCRETE MILLINGS, FLOATING DEBRIS, PAPER, PLASTIC, FABRIC, CONSTRUCTION & DEMOLITION DEBRIS & OTHER WASTES MUST BE DISPOSED OF PROPERLY & MUST COMPLY WITH MFRA DISPOSAL REQUIREMENTS.
 16. HAZARDOUS MATERIALS: OIL, GASOLINE, PAINT & ANY HAZARDOUS SUBSTANCES MUST BE PROPERLY STORED, INCLUDING SECONDARY CONTAINMENT, TO PREVENT SPILLS, LEAKS OR OTHER DISCHARGE. RESTRICTED ACCESS TO STORAGE AREAS MUST BE PROVIDED TO PREVENT VANDALISM. STORAGE & DISPOSAL OF HAZARDOUS WASTE MUST BE IN COMPLIANCE WITH MFRA REGULATIONS.
 17. ALL STORM WATER POLLUTION PREVENTION MEASURES PRESENTED ON THIS PLAN, AND IN THE SWPPP, SHALL BE INITIATED AS SOON AS PRACTICABLE AND PRIOR TO SOIL DISTURBING ACTIVITIES UPSLOPE.
 18. DISTURBED PORTIONS OF THE SITE WITHIN 200 FEET OF AND FLOWING TO A SURFACE WATER, AS DEFINED IN THE GENERAL PERMIT, WHERE CONSTRUCTION ACTIVITY HAS STOPPED SHALL BE TEMPORARILY SEED. SEEDING SHALL BE IN ACCORDANCE WITH MN/DOT SEED MIXTURE NUMBER 100 OR 110 DEPENDING ON THE SEASON OF PLANTING (SEE MN/DOT SPECIFICATION SECTION 2575.3) SEEDING METHOD AND APPLICATION RATE SHALL CONFORM TO MN/DOT SPECIFICATION SECTION 2575.3, TIMING OF TEMPORARY SEEDING SHALL BE IN ACCORDANCE WITH THE FOLLOWING GUIDELINES:
- | TYPE OF SLOPE | TIME (MAX TIME AN AREA CAN REMAIN OPEN WHEN THE AREA IS NOT ACTIVELY BEING WORKED) |
|-------------------|--|
| STEEPER THAN 3:1 | 7 DAYS |
| 10:1 TO 3:1 | 14 DAYS |
| FLATTER THAN 10:1 | 21 DAYS |
- DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITY HAS PERMANENTLY STOPPED SHALL BE PERMANENTLY SEED. THESE AREAS SHALL BE SEED IN ACCORDANCE WITH THE TIME TABLE DESCRIBED ABOVE. REFER TO THE GRADING PLAN AND/OR LANDSCAPE PLAN FOR VEGETATIVE COVER.
19. CONTRACTORS OR SUBCONTRACTORS WILL BE RESPONSIBLE FOR REMOVING SEDIMENT FROM CONVEYANCES & FROM TEMPORARY SEDIMENTATION BASINS THAT ARE TO BE USED AS PERMANENT WATER QUALITY MANAGEMENT BASINS. SEDIMENT MUST BE STABILIZED TO PREVENT IT FROM BEING WASHED BACK INTO THE BASIN, CONVEYANCES, OR DRAINAGEWAYS DISCHARGING OFF-SITE OR TO SURFACE WATERS. THE CLEANOUT OF PERMANENT BASINS MUST BE SUFFICIENT TO RETURN THE BASIN TO DESIGN CAPACITY.
 20. ON-SITE & OFF-SITE SOIL STOCKPILE AND BORROW AREAS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION THROUGH IMPLEMENTATION OF BMP'S. STOCKPILE AND BORROW AREA LOCATIONS SHALL BE NOTED ON THE SITE MAP AND PERMITTED IN ACCORDANCE WITH GENERAL PERMIT REQUIREMENTS.
 21. TEMPORARY SOIL STOCKPILES MUST HAVE SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROLS & CANNOT BE PLACED IN SURFACE WATERS, INCLUDING STORMWATER CONVEYANCES SUCH AS CURB & GUTTER SYSTEMS OR CONDUITS & DITCHES.
 22. SLOPES SHALL BE LEFT IN A ROUGHENED CONDITION DURING THE GRADING PHASE TO REDUCE RUNOFF VELOCITIES AND EROSION.
 23. DUE TO THE GRADE CHANGES DURING THE DEVELOPMENT OF THE PROJECT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE EROSION CONTROL MEASURES (SILT FENCES, CHECK DAMS, INLET PROTECTION DEVICES, ETC.) TO PREVENT EROSION.
 24. ALL CONSTRUCTION SHALL BE STABILIZED AT THE END OF EACH WORKING DAY, THIS INCLUDES BACKFILLING OF TRENCHES FOR UTILITY CONSTRUCTION AND PLACEMENT OF GRAVEL OR BITUMINOUS PAVING FOR ROAD CONSTRUCTION.

MAINTENANCE

- ALL MEASURES STATED ON THIS EROSION AND SEDIMENT CONTROL PLAN, AND IN THE STORM WATER POLLUTION PREVENTION PLAN SHALL BE MAINTAINED IN FULLY FUNCTIONAL CONDITION UNTIL NO LONGER REQUIRED FOR A COMPLETED PHASE OF WORK OR FINAL STABILIZATION OF THE SITE. THE DESIGNATED CONTACT PERSON NOTED ON THIS PLAN MUST ROUTINELY INSPECT THE CONSTRUCTION ON SITE ONCE EVERY SEVEN DAYS DURING ACTIVE CONSTRUCTION AND WITHIN 24 HOURS AFTER A RAINFALL EVENT GREATER THAN 0.5 INCHES IN 24 HOURS. ALL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE CLEANED AND REPAIRED IN ACCORDANCE WITH THE FOLLOWING:
1. ALL SILT FENCES MUST BE REPAIRED, REPLACED, OR SUPPLEMENTED WHEN THEY BECOME NONFUNCTIONAL OR THE SEDIMENT REACHES 1/3 OF THE HEIGHT OF THE FENCE. THESE REPAIRS MUST BE MADE WITHIN 24 HOURS OF DISCOVERY, OR AS SOON AS FIELD CONDITIONS ALLOW ACCESS.
 2. TEMPORARY AND PERMANENT SEDIMENTATION BASINS MUST BE DRAINED AND THE SEDIMENT REMOVED WHEN THE DEPTH OF SEDIMENT COLLECTED IN THE BASIN REACHES 1/2 THE STORAGE VOLUME. DRAINAGE AND REMOVAL MUST BE COMPLETED WITHIN 72 HOURS OF DISCOVERY, OR AS SOON AS FIELD CONDITIONS ALLOW ACCESS (SEE PART IV.D. OF THE GENERAL PERMIT).
 3. SURFACE WATERS, INCLUDING DRAINAGE DITCHES AND CONVEYANCE SYSTEMS, MUST BE INSPECTED FOR EVIDENCE OF SEDIMENT BEING DEPOSITED BY EROSION. THE CONTRACTOR MUST REMOVE ALL DELTAS AND SEDIMENT DEPOSITED IN SURFACE WATERS, INCLUDING DRAINAGE WAYS, CATCH BASINS, AND OTHER DRAINAGE SYSTEMS, AND RESTABILIZE THE AREAS WHERE SEDIMENT REMOVAL RESULTS IN EXPOSED SOIL. THE REMOVAL AND STABILIZATION MUST TAKE PLACE WITHIN SEVEN (7) DAYS OF DISCOVERY UNLESS PRECLUDED BY LEGAL, REGULATORY, OR PHYSICAL ACCESS CONSTRAINTS. THE CONTRACTOR SHALL USE ALL REASONABLE EFFORTS TO OBTAIN ACCESS. IF PRECLUDED, REMOVAL AND STABILIZATION MUST TAKE PLACE WITHIN SEVEN (7) CALENDAR DAYS OF OBTAINING ACCESS. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING ALL LOCAL, REGIONAL, STATE AND FEDERAL AUTHORITIES AND RECEIVING ANY APPLICABLE PERMITS, PRIOR TO CONDUCTING ANY WORK.
 4. CONSTRUCTION SITE VEHICLE EXIT LOCATIONS MUST BE INSPECTED FOR EVIDENCE OF OFF-SITE SEDIMENT TRACKING ONTO PAVED SURFACES. TRACKED SEDIMENT MUST BE REMOVED FROM ALL OFF-SITE PAVED SURFACES, WITHIN 24 HOURS OF DISCOVERY, OR IF APPLICABLE, WITHIN A SHORTER TIME TO COMPLY WITH PART IV.C.6 OF THE GENERAL PERMIT.
 5. THE CONTRACTOR IS RESPONSIBLE FOR THE OPERATION AND MAINTENANCE OF TEMPORARY AND PERMANENT WATER QUALITY MANAGEMENT BMP'S, AS WELL AS ALL EROSION PREVENTION AND SEDIMENT CONTROL BMP'S, FOR THE DURATION OF THE CONSTRUCTION WORK AT THE SITE. THE PERMITEE(S) ARE RESPONSIBLE UNTIL ANOTHER PERMITEE HAS ASSUMED CONTROL ACCORDING TO PART II.B.5 OVER ALL AREAS OF THE SITE THAT HAVE NOT BEEN FINALLY STABILIZED OR THE SITE HAS UNDERGONE FINAL STABILIZATION, AND A NOT HAS BEEN SUBMITTED TO THE MFRA.
 6. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFF-SITE ACCUMULATIONS OF SEDIMENT MUST BE REMOVED IN A MANNER AND AT A FREQUENCY SUFFICIENT TO MINIMIZE OFF-SITE IMPACTS (E.G., FUGITIVE SEDIMENT IN STREETS COULD BE WASHED INTO STORM SEWERS BY THE NEXT RAIN AND/OR POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS).
 7. ALL INFILTRATION AREAS MUST BE INSPECTED TO ENSURE THAT NO SEDIMENT FROM ONGOING CONSTRUCTION ACTIVITIES IS REACHING THE INFILTRATION AREA AND THESE AREAS ARE PROTECTED FROM COMPACTION DUE TO CONSTRUCTION EQUIPMENT DRIVING ACROSS THE INFILTRATION AREA.

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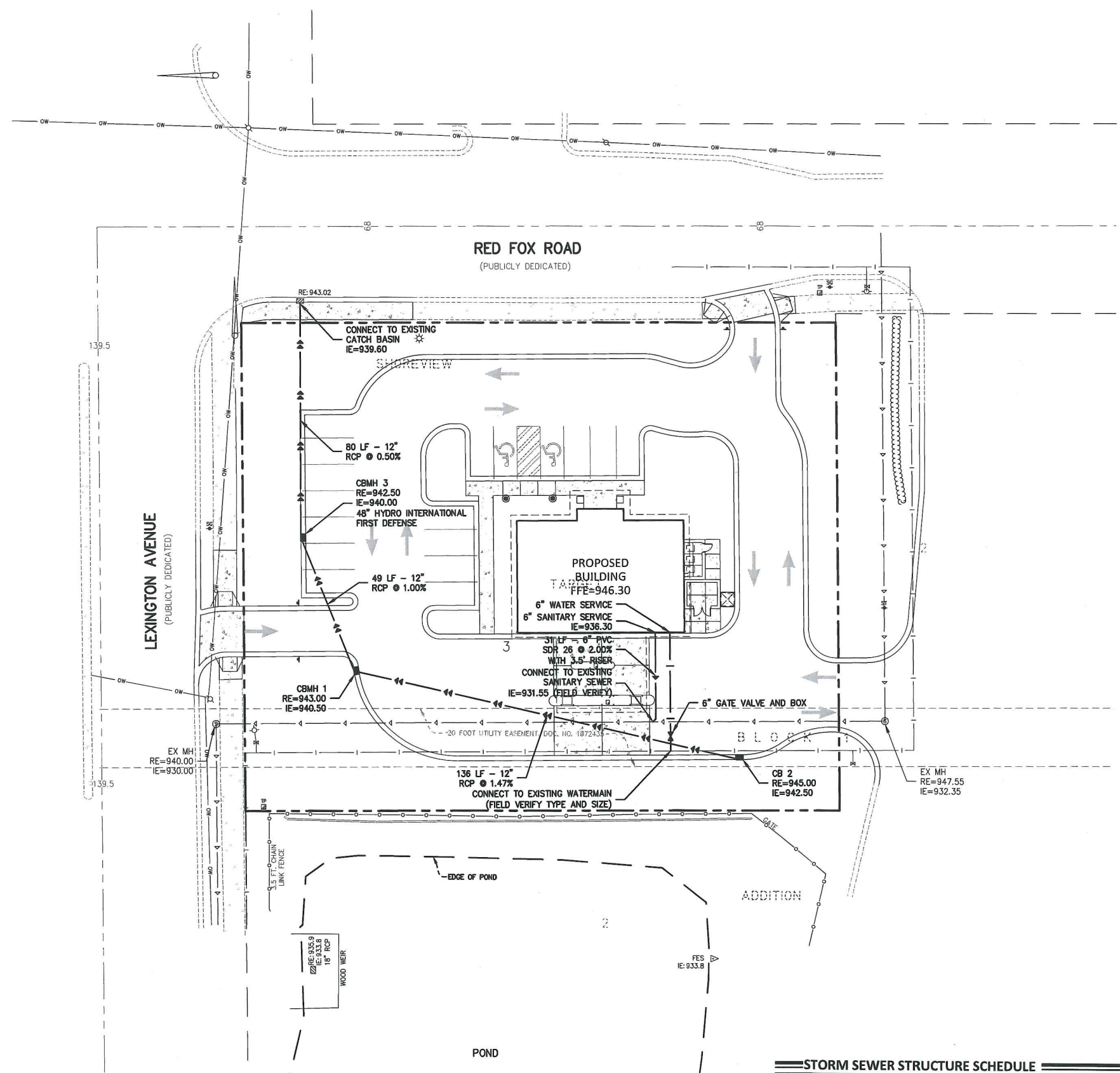
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EROSION
CONTROL
DETAILS AND
NOTES
Sheet No. Revision
C5.03 A

Project No. HTG19272



LEGEND

	PROPOSED	EXISTING
CURB & GUTTER	—	—
STORM SEWER	—	—
SANITARY SEWER	—	—
FORCEMAIN (SAN.)	—	—
WATERMAIN	—	—
EASEMENT	—	—
DRAINTILE	—	—
GAS LINE	—	—
ELECTRIC	—	—
TELEPHONE	—	—

UTILITY CONSTRUCTION NOTES

THE UTILITY IMPROVEMENTS FOR THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE "STANDARD UTILITIES SPECIFICATIONS" AS PUBLISHED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM), EXCEPT AS MODIFIED HEREIN. CONTRACTOR SHALL OBTAIN A COPY OF THESE SPECIFICATIONS.

- ALL UTILITIES SHALL BE CONSTRUCTED IN ACCORDANCE TO CITY REQUIREMENTS.
- CONTRACTOR SHALL NOT OPEN, TURN OFF, INTERFERE WITH, OR ATTACH ANY PIPE OR HOSE TO OR TAP WATERMAIN BELONGING TO THE CITY UNLESS DULY AUTHORIZED TO DO SO BY THE CITY. ANY ADVERSE CONSEQUENCES OF ANY SCHEDULED OR UNSCHEDULED DISRUPTIONS OF SERVICE TO THE PUBLIC ARE THE LIABILITY OF THE CONTRACTOR.
- A MINIMUM VERTICAL SEPARATION OF 18 INCHES IS REQUIRED AT ALL WATERMAIN AND SEWER MAIN CROSSINGS.
- ALL MATERIALS SHALL BE AS SPECIFIED IN CEAM SPECIFICATIONS EXCEPT AS MODIFIED HEREIN.
- ALL MATERIALS SHALL COMPLY WITH THE REQUIREMENTS OF THE CITY.
- ALL SANITARY SEWER TO BE PVC SDR-35, UNLESS NOTED OTHERWISE.
- ALL WATERMAIN TO BE DUCTILE IRON - CLASS 52, WITH 7.5 FEET MINIMUM COVER.
- ALL STORM SEWER PIPE TO BE REINFORCED CONCRETE PIPE WITH R-4 JOINTS, AND RUBBER GASKETS.
- RIP RAP SHALL BE Mn/DOT CLASS 3.
- CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULE, EXIT PORCHES, RAMPS, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS. THE LOCATIONS OF SMALL UTILITIES SHALL BE OBTAINED BY THE CONTRACTOR, BY CALLING GOPHER STATE ONE CALL AT 454-0002.
- THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT. THE CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR ANY DAMAGES TO THE ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASES OF THIS PROJECT.
- SAFETY NOTICE TO CONTRACTORS: IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS ON THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. THE DUTY OF THE ENGINEER OR THE DEVELOPER TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES IN, ON OR NEAR THE CONSTRUCTION SITE.
- ALL AREAS OUTSIDE THE PROPERTY BOUNDARIES THAT ARE DISTURBED BY UTILITY CONSTRUCTION SHALL BE RESTORED IN KIND. SODDED AREAS SHALL BE RESTORED WITH 6 INCHES OF TOPSOIL PLACED BENEATH THE SOD.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. TRAFFIC CONTROL DEVICES SHALL CONFORM TO APPROPRIATE MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARDS.
- ALL SOILS TESTING SHALL BE COMPLETED BY AN INDEPENDENT SOILS ENGINEER. EXCAVATION FOR THE PURPOSE OF REMOVING UNSTABLE OR UNSUITABLE SOILS SHALL BE COMPLETED AS REQUIRED BY THE SOILS ENGINEER. THE UTILITY BACKFILL CONSTRUCTION SHALL COMPLY WITH THE REQUIREMENTS OF THE SOILS ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOILS TESTS AND SOIL INSPECTIONS WITH THE SOILS ENGINEER.

A GEOTECHNICAL ENGINEERING REPORT HAS BEEN COMPLETED BY:
 COMPANY: TBD
 ADDRESS:
 PHONE:
 DATED:

THE CONTRACTOR SHALL OBTAIN A COPY OF THIS SOILS REPORT.

- PRIOR TO PLACEMENT OF AGGREGATE BASE, A TEST ROLL WILL BE REQUIRED ON THE STREET AND PARKING AREA SUBGRADE. THE CONTRACTOR SHALL PROVIDE A LOADED TANDUM AXLE TRUCK WITH A GROSS WEIGHT OF 25 TONS. THE TEST ROLLING SHALL BE AT THE DIRECTION OF THE SOILS ENGINEER AND SHALL BE COMPLETED IN AREAS AS DIRECTED BY THE SOILS ENGINEER. THE SOILS ENGINEER SHALL DETERMINE WHICH SECTIONS OF THE STREET OR PARKING AREA ARE UNSTABLE. CORRECTION OF THE SUBGRADE SOILS SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SOILS ENGINEER.
- THE TREES AND OTHER NATURAL VEGETATION WITHIN THE PROJECT AND/OR ADJACENT TO THE PROJECT ARE OF PRIME CONCERN TO THE CONTRACTOR'S OPERATIONS. HE WILL BE REQUIRED TO PROTECT THE TREES WHICH ARE TO BE SAVED TO BE SURE THAT EQUIPMENT IS NOT NEEDLESSLY OPERATED UNDER NEARBY TREES AND SHALL EXERCISE EXTREME CAUTION IN WORKING ADJACENT TO TREES. SHOULD ANY PORTION OF THE TREE BRANCHES REQUIRE REMOVAL TO PERMIT OPERATION OF THE CONTRACTOR'S EQUIPMENT, HE SHALL OBTAIN THE SERVICES OF A PROFESSIONAL TREE TRIMMING SERVICE TO TRIM THE TREES PRIOR TO THE BEGINNING OF THE OPERATION. SHOULD THE CONTRACTORS' OPERATIONS RESULT IN THE BREAKING OF ANY LIMBS, THE BROKEN LIMBS SHOULD BE REMOVED IMMEDIATELY AND CUTS SHALL BE PROPERLY PROTECTED TO MINIMIZE ANY AUTHORIZATION BY THE ENGINEER. COSTS FOR TRIMMING SERVICES SHALL BE CONSIDERED INCIDENTAL TO THE GRADING CONSTRUCTION AND NO SPECIAL PAYMENT WILL BE MADE.

STORM SEWER STRUCTURE SCHEDULE

NUMBER	TYPE	CASTING*
CBMH1	CATCH BASIN MANHOLE	R-3067-V
CB2	CATCH BASIN	R-3067-V
CBMH3	TREATMENT DEVICE	R-3067-V

*INDICATES NEENAH FOUNDRY CASTING NO., OR APPROVED EQUAL
 NOTE: THE CONTRACTOR AND THEIR SUPPLIER SHALL DETERMINE THE MINIMUM DIAMETER REQUIRED FOR EACH STORM SEWER STRUCTURE.

engineering surveying planning energy

14800 28th Ave. N, Ste 140
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 (763) 476.6010 telephone
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Client
HTG ARCHITECTS
 9300 HENNEPIN TOWN ROAD
 MINNEAPOLIS, MN 55347

Project
TCF BANK

Location
SHOREVIEW, MINNESOTA

Certification
 I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed professional engineer under the laws of the state of Minnesota.

Brad C. Wenzel
 Registration No. 26908 Date: 05/08/2012
 If available, contact us for a wet signed copy of this plan which is available upon request at MFRA, Inc., Plymouth, MN office.

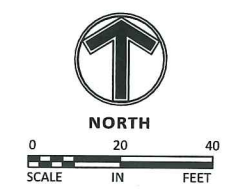
Summary
 Designed: bcw Drawn: JN
 Approved: MCA Book / Page:
 Phase: PRELIM Initial Issue: 04/30/2012

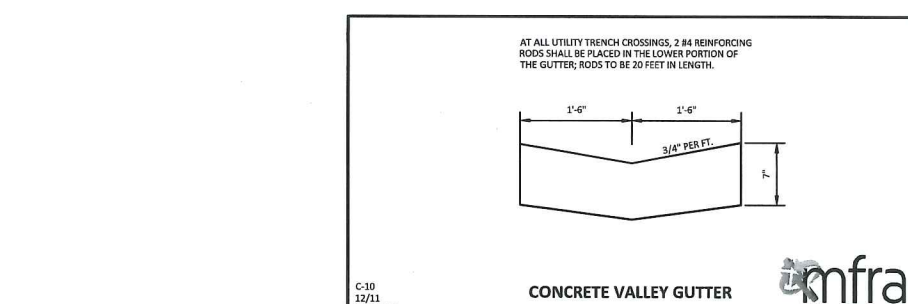
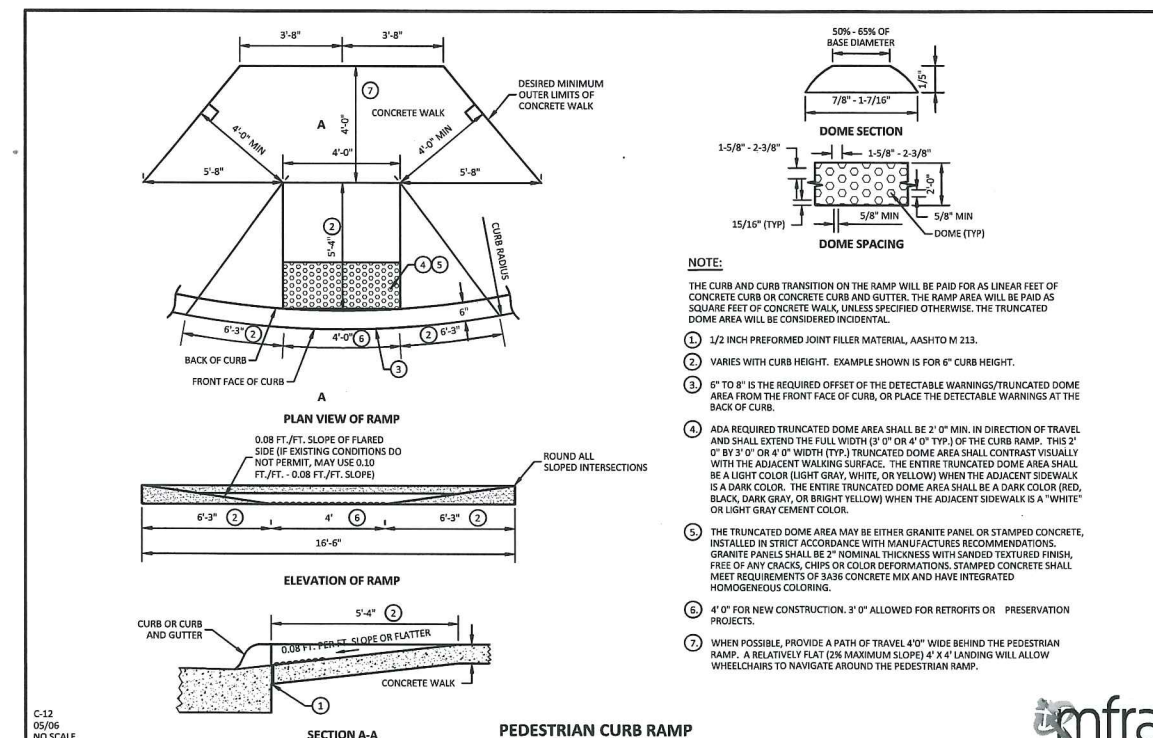
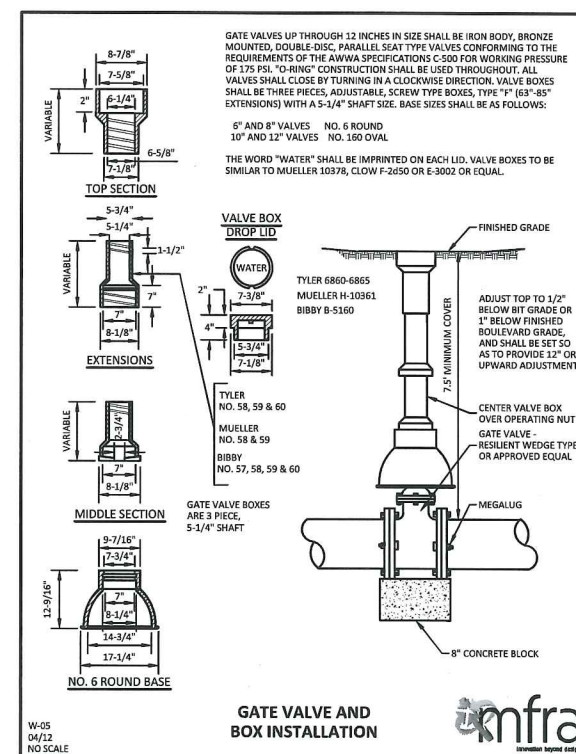
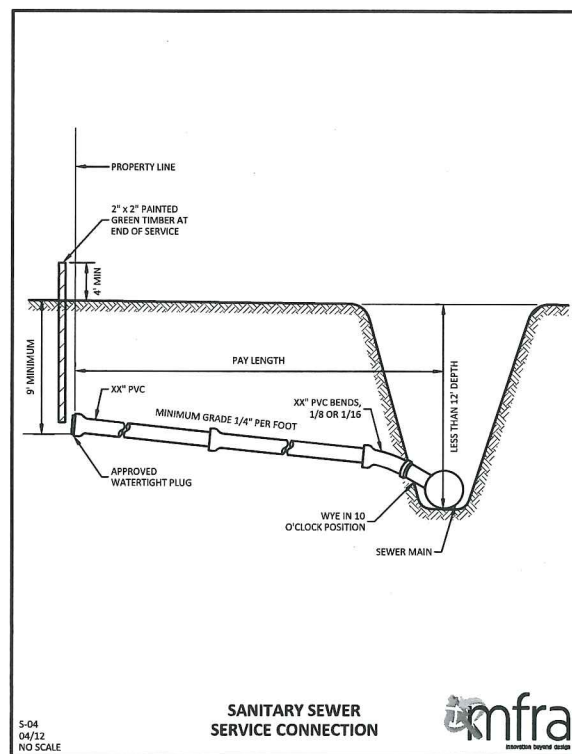
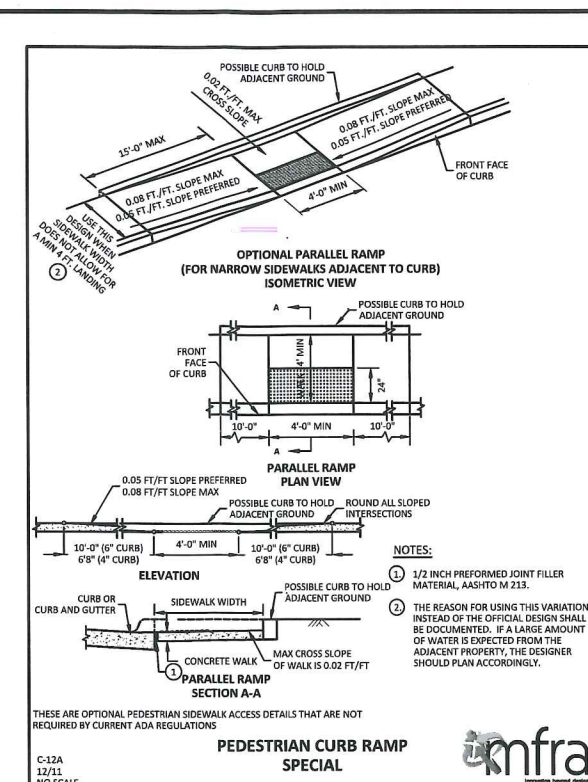
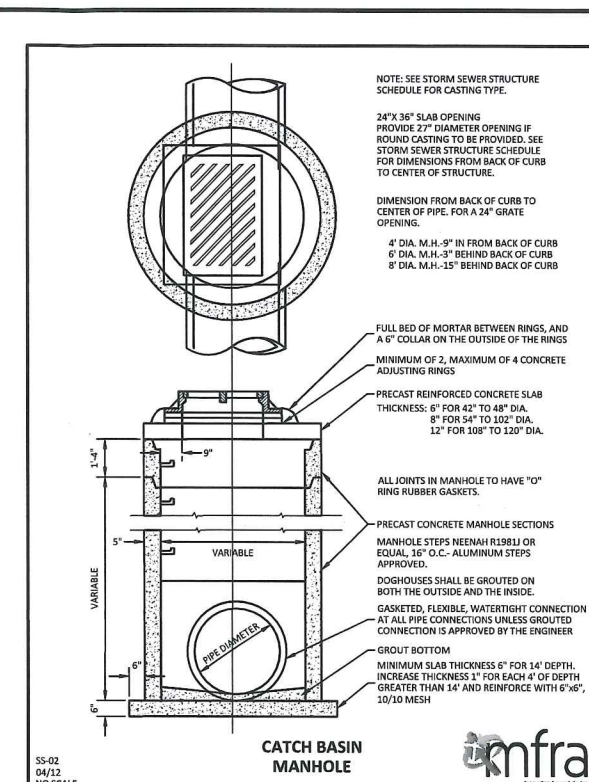
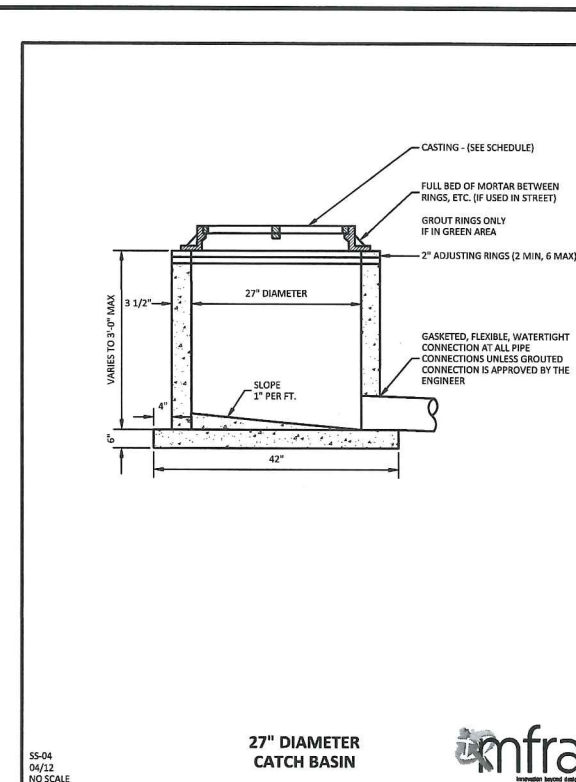
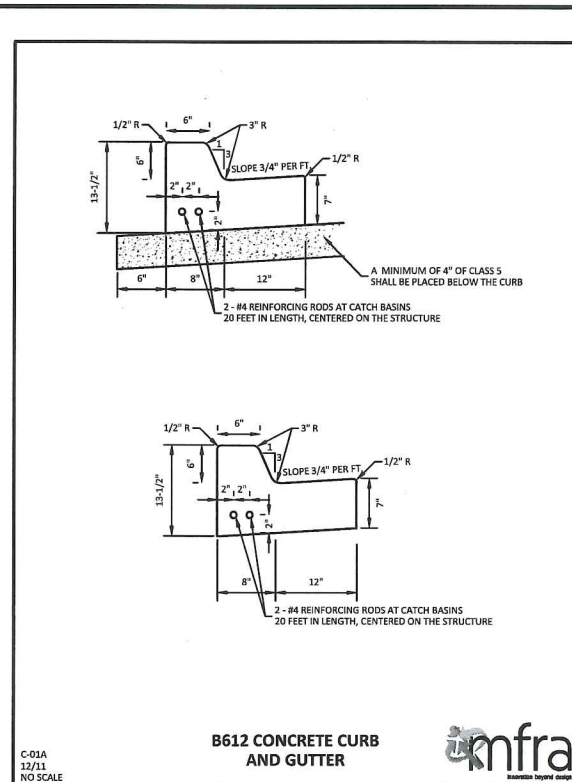
Revision History
 No. Date By Submittal / Revision
 A 05/08/12 JN CITY COMMENTS

Sheet Title
UTILITY PLAN

Sheet No. Revision
C6.01 A

Project No. HTG19272





Certification

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed professional ENGINEER under the laws of the state of Minnesota.

Brad C. Hennings
Registration No. 26908 Date: 05/08/2012
If available, contact us for a wet signed copy of this plan, which is available upon request at MFRA, Inc., Plymouth, MN office.

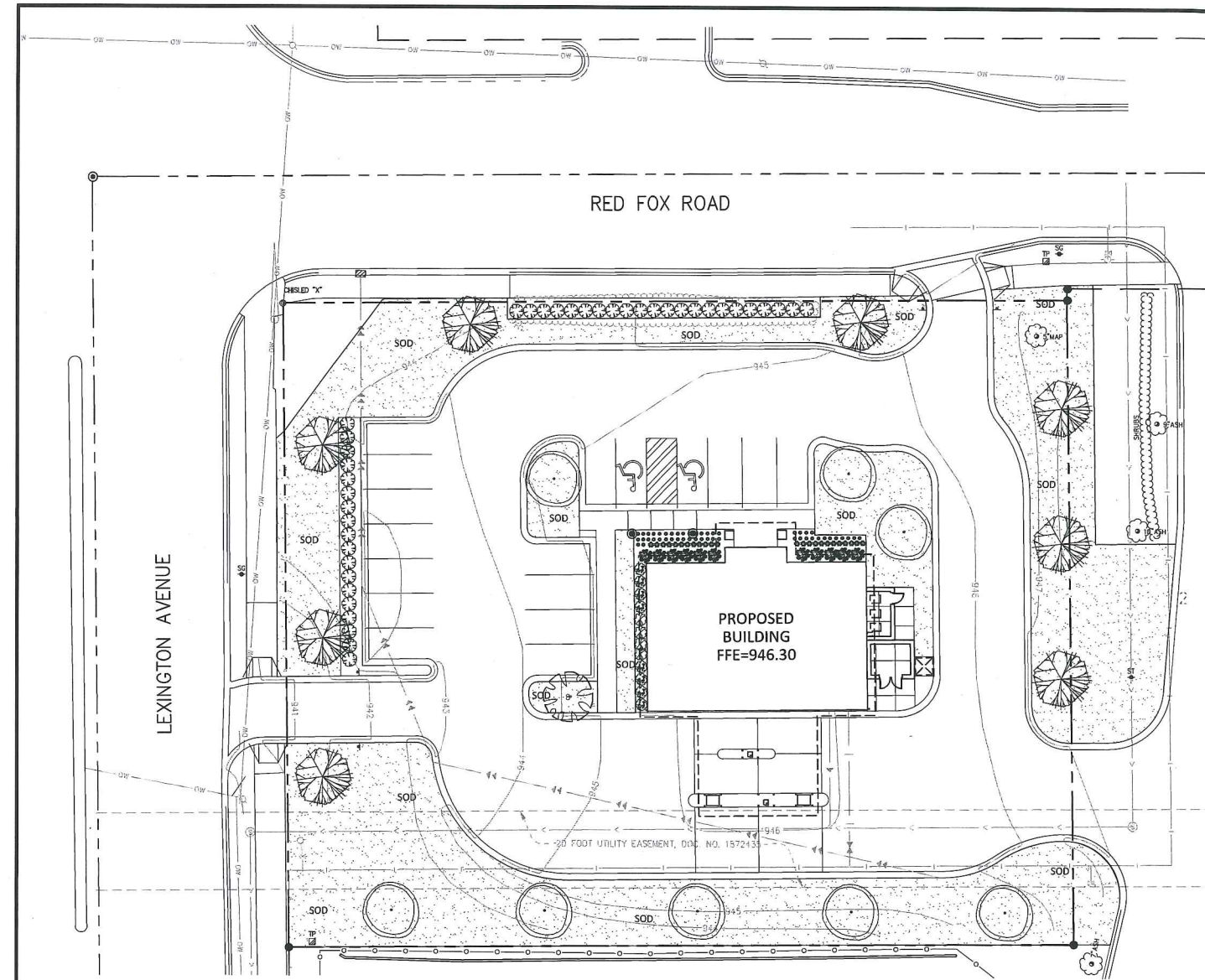
Summary

Designed: B/CW Drawn: J/N
Approved: MCA Book / Page:
Phase: PRELIM Initial Issue: 04/30/2012

Revision History

No.	Date	By	Submittal / Revision
A	05/08/12	J/N	CITY COMMENTS

Sheet Title
CONSTRUCTION DETAILS



LANDSCAPE NOTES

- ALL PLANTS MUST BE HEALTHY, VIGOROUS MATERIAL, FREE OF PESTS AND DISEASE AND BE CONTAINER GROWN OR BALLED AND BURLAPPED AS INDICATED IN THE LANDSCAPE LEGEND.
- ALL TREES MUST BE STRAIGHT TRUNKED AND FULL HEADED AND MEET ALL REQUIREMENTS SPECIFIED.
- THE LANDSCAPE ARCHITECT RESERVES THE RIGHT TO REJECT ANY PLANTS WHICH ARE DEEMED UNSATISFACTORY BEFORE, DURING, OR AFTER INSTALLATION.
- NO SUBSTITUTIONS OF PLANT MATERIAL SHALL BE ACCEPTED UNLESS APPROVED IN WRITING BY THE LANDSCAPE ARCHITECT.
- ALL PLANTING STOCK SHALL CONFORM TO THE "AMERICAN STANDARD FOR NURSERY STOCK" ANSI-Z60, LATEST EDITION, OF THE AMERICAN ASSOCIATION OF NURSERYMEN, INC. AND SHALL CONSTITUTE MINIMUM QUALITY REQUIREMENTS FOR PLANT MATERIALS.
- EXISTING TREES AND SHRUBS TO REMAIN SHALL BE PROTECTED TO THE DRIP LINE FROM ALL CONSTRUCTION TRAFFIC, STORAGE OF MATERIALS ETC. WITH 4" HT. ORANGE PLASTIC SAFETY FENCING ADEQUATELY SUPPORTED BY STEEL FENCE POSTS @ 6" O.C. MAXIMUM SPACING.
- ALL PLANT MATERIAL QUANTITIES, SHAPES OF BEDS AND LOCATIONS SHOWN ARE APPROXIMATE. CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETE COVERAGE OF ALL PLANTING BEDS AT SPACING SHOWN AND ADJUSTED TO CONFORM TO THE EXACT CONDITIONS OF THE SITE. THE LANDSCAPE ARCHITECT SHALL APPROVE THE STAKING LOCATION OF ALL PLANT MATERIALS PRIOR TO INSTALLATION.
- ALL TREES MUST BE PLANTED, MULCHED, AND STAKED AS SHOWN IN THE DETAILS.
- ALL PLANTING AREAS MUST BE COMPLETELY MULCHED AS SPECIFIED.
- MULCH: DOUBLE SHREDDED DARK HARDWOOD MULCH, CLEAN AND FREE OF NOXIOUS WEEDS OR OTHER DELETERIOUS MATERIAL. IN ALL STAND ALONE TREE PITS, SHRUB BEDS AND MONUMENT SIGNS SHALL BE ROCK MULCH. SUBMIT SAMPLE TO LANDSCAPE ARCHITECT PRIOR TO DELIVERY ON-SITE FOR APPROVAL. DELIVER MULCH ON DAY OF INSTALLATION. USE 4" FOR TREES UNLESS OTHERWISE DIRECTED.
- STEEL LANDSCAPE EDGING: SHALL BE "COMET" EDGING SHALL BE MODEL #1007 (10X76GA). COLOR SHALL BE BLACK.
- THE PLAN TAKES PRECEDENCE OVER THE LANDSCAPE LEGEND IF DISCREPANCIES EXIST. THE SPECIFICATIONS TAKE PRECEDENCE OVER THE PLANTING NOTES AND GENERAL NOTES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL MULCHES AND PLANTING SOIL QUANTITIES TO COMPLETE THE WORK SHOWN ON THE PLAN. VERIFY ALL QUANTITIES SHOWN ON THE LANDSCAPE LEGEND.
- LONG-TERM STORAGE OF MATERIALS OR SUPPLIES ON-SITE WILL NOT BE ALLOWED.
- THE CONTRACTOR SHALL KEEP PAVEMENTS, PLANTERS AND BUILDINGS CLEAN AND UNSTAINED. ALL PEDESTRIAN AND VEHICLE ACCESS TO BE MAINTAINED THROUGHOUT CONSTRUCTION PERIOD. ALL WASTES SHALL BE PROMPTLY REMOVED FROM THE SITE. ANY PLANT STOCK NOT PLANTED ON DAY OF DELIVERY SHALL BE HELED IN AND WATERED UNTIL INSTALLATION. PLANTS NOT MAINTAINED IN THIS MANNER WILL BE REJECTED. ANY DAMAGE TO EXISTING FACILITIES SHALL BE REPAVED AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE CODES, REGULATIONS, AND PERMITS GOVERNING THE WORK.
- LOCATE AND VERIFY ALL UTILITIES, INCLUDING IRRIGATION LINES, WITH THE OWNER FOR PROPRIETARY UTILITIES AND CIPHER STATE ONE CALL AT 454-0002 (TWIN CITIES METRO AREA) OR 800-252-1166 (GREATER MINNESOTA) 48 HOURS BEFORE DIGGING. CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND REPAIR OF ANY DAMAGES TO SAME. NOTIFY THE LANDSCAPE ARCHITECT OF ANY CONFLICTS TO FACILITATE PLANT RELOCATION.
- USE ANTI-DESICCANT (WILTPROF OR APPROVED EQUAL) ON DECIDUOUS PLANTS MOVED IN LEAF AND FOR EVERGREENS MOVED ANYTIME. APPLY AS PER MANUFACTURER'S INSTRUCTION. ALL EVERGREENS SHALL BE SPRAYED IN THE LATE FALL FOR WINTER PROTECTION DURING WARRANTY PERIOD.
- PLANTING SOIL FOR TREES, SHRUBS AND GROUND COVERS: FERTILE FRIABLE LOAM CONTAINING A LIBERAL AMOUNT OF HUMUS AND CAPABLE OF SUSTAINING VIGOROUS PLANT GROWTH. IT SHALL COMPLY WITH MN/DOT SPECIFICATION 3877 TYPE B SELECT TOPSOIL. MIXTURE SHALL BE FREE FROM HARDPOCK SUBSOIL, STONES, CHEMICALS, NOXIOUS WEEDS, ETC. SOIL MIXTURE SHALL HAVE A PH BETWEEN 6.1 AND 7.5 AND 10-0-10 FERTILIZER AT THE RATE OF 3 POUNDS PER CUBIC YARD. IN PLANTING BEDS INCORPORATE THIS MIXTURE THROUGHOUT THE ENTIRE BED BY ROTOTILLING IT INTO THE TOP 12" OF SOIL.
- ALL PLANTS SHALL BE GUARANTEED FOR TWO COMPLETE GROWING SEASONS (APRIL 1 - NOVEMBER 1), UNLESS OTHERWISE SPECIFIED. THE GUARANTEE SHALL COVER THE FULL COST OF REPLACEMENT INCLUDING LABOR AND PLANTS.
- CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT AT LEAST 3 DAYS PRIOR TO PLANNED DELIVERY. THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT AT LEAST 24 HOURS IN ADVANCE OF BEGINNING PLANT INSTALLATION.
- SEASON/TIME OF PLANTING AND SEEDING: NOTE: THE CONTRACTOR MAY ELECT TO PLANT IN OFF-SEASONS ENTIRELY AT HIS/HER RISK.
DECIDUOUS POTTED PLANTS: APRIL 1-JUNE 1; AUG. 21-NOV. 1
DECIDUOUS B&B: APRIL 1-JUNE 1; AUG. 21-NOV. 1
EVERGREEN POTTED PLANTS: APRIL 1-JUNE 1; AUG. 21-OCT. 1
EVERGREEN B&B: APRIL 1-MAY 15; AUG. 21-SEPT. 15
- MAINTENANCE SHALL BEGIN IMMEDIATELY AFTER EACH PORTION OF THE WORK IS IN PLACE. PLANT MATERIAL SHALL BE PROTECTED AND MAINTAINED UNTIL THE INSTALLATION OF THE PLANTS IS COMPLETE. INSPECTION HAS BEEN MADE, AND PLANTINGS ARE ACCEPTED EXCLUSIVE OF THE GUARANTEE. MAINTENANCE SHALL INCLUDE WATERING, CULTIVATING, MULCHING, REMOVAL OF DEAD MATERIALS, RE-SETTING PLANTS TO PROPER GRADE AND KEEPING PLANTS IN A PLUMB POSITION. AFTER ACCEPTANCE, THE OWNER SHALL ASSUME MAINTENANCE RESPONSIBILITIES. HOWEVER, THE CONTRACTOR SHALL CONTINUE TO BE RESPONSIBLE FOR KEEPING THE TREES PLUMB THROUGHOUT THE GUARANTEE PERIOD.
- ANY PLANT MATERIAL WHICH DIES, TURNS BROWN, OR DEFOLIATES (PRIOR TO TOTAL ACCEPTANCE OF THE WORK) SHALL BE PROMPTLY REMOVED FROM THE SITE AND REPLACED WITH MATERIAL OF THE SAME SPECIES, QUANTITY, AND SIZE AND MEETING ALL LANDSCAPE LEGEND SPECIFICATIONS.
- WATERING: MAINTAIN A WATERING SCHEDULE WHICH WILL THOROUGHLY WATER ALL PLANTS ONCE A WEEK. IN EXTREMELY HOT, DRY WEATHER, WATER MORE OFTEN AS REQUIRED BY INDICATIONS OF HEAT STRESS SUCH AS WILTING LEAVES. CHECK MOISTURE UNDER MULCH PRIOR TO WATERING TO DETERMINE NEED. CONTRACTOR SHALL MAKE THE NECESSARY ARRANGEMENTS FOR WATER.
- CONTRACTOR SHALL REQUEST IN WRITING, A FINAL ACCEPTANCE INSPECTION.
- ALL DISTURBED AREAS TO BE SODDED, ARE TO RECEIVE 4" TOP SOIL, AND WATER UNTIL A HEALTHY STAND OF GRASS IS OBTAINED.
- IRRIGATION SYSTEM TO BE DESIGN BUILD, SUBMIT DRAWINGS FOR PRIOR APPROVAL. IRRIGATE ALL SOD WITH EITHER SPRAYS OR ROTATORS AT A MINIMUM DESIGN OF 1" PER WEEK. IRRIGATE ALL PLANTERS WITH A DRIP SYSTEM AT 25" PER WEEK. INSTALL A TOTAL OF 4 QUICK COUPLERS AT THE CORNERS OF THE PROPERTY. A 2.5" TYPE K SOURCE PIPE IS PROVIDED BY MECHANICAL.
- CONTRACTOR SHALL IRRIGATE THE ENTIRE PROPERTY.

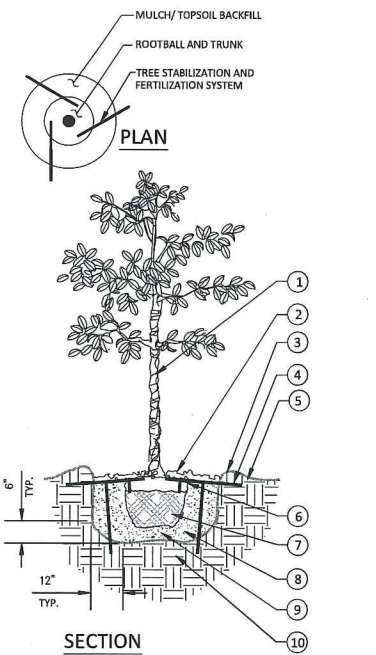
LANDSCAPE LEGEND

KEY	BOTANICAL NAME	COMMON NAME	SIZE	ROOT	QTY.	INSTALL HEIGHT	MATURE HEIGHT
DECIDUOUS TREES							
	Acer saccharum	SUGAR MAPLE	2.5"	BB	8	15'	50'
	Tilia americana 'Boulevard'	BOULEVARD LINDEN	2.5"	BB	8	15'	60'
	Celtis occidentalis	HACKBERRY	2.5"	BB	1	14'	55'
SHRUBS							
	Ribes alpinum	ALPINE CURRENT	#5	CONT.	40	1'	3'x4'
	Spiraea x bumalda 'Anthony Waterer'	ANTHONY WATERER SPIREA	#5	CONT.	8	1.5'	3'x4'
	Amelanchier alnifolia 'Regent'	SERVICEBERRY	#5	CONT.	11	1'	3'x4'
	Spiraea x bumalda 'Gold Flame'	GOLD FLAME SPIREA	#5	CONT.	4	1.5'	3'x4'
PERENNIALS							
	Hemerocallis 'Baja'	BAJA DAYLILY	#1	CONT.	25	6"	2'x1.5'
	Hemerocallis 'Stella d'Oro'	STELLA D'ORO DAYLILY	#1	CONT.	49	6"	1.5'x1.5'

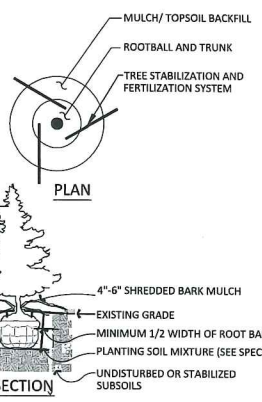
QUANTITIES SHOWN IN THE PLANTING SCHEDULE ARE FOR THE CONTRACTOR'S CONVENIENCE. CONTRACTOR TO VERIFY QUANTITIES SHOWN ON THE PLAN.
PER CITY CODE, TWO (2) TREES ARE REQUIRED FOR PARKING STALL QUANTITIES AND FOURTEEN (14) TREES ARE REQUIRED FOR THE 34 CAL. IN. OF EXISTING TREES REMOVED.

SOD & MULCH LEGEND

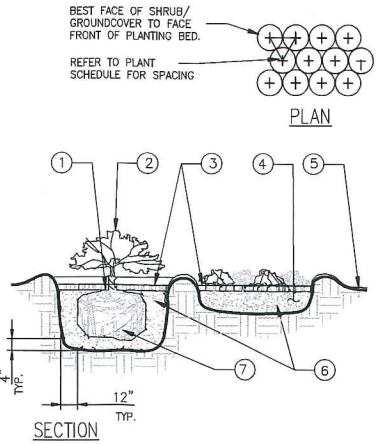
SYM.	TYPE
	DOUBLE SHREDDED DARK HARDWOOD MULCH
	SOD
	HIGHLAND



- TREE WRAP
 - 4" MINIMUM OF HARDWOOD BARK MULCH COMPACTED OR AS SPECIFIED.
 - 3" HIGH SOIL BERM TO HOLD WATER.
 - STAKE SIZE SHALL BE ONE SIZE HIGHER THAN REQUIRED FOR SIZE OF TREE. REFER TO SITEWORK SPECIFICATIONS FOR APPROVED MATERIALS AND INSTALLATION REQUIREMENTS.
 - FINISHED GRADE (SEE GRADING PLAN)
 - TOP OF ROOTBALL MIN. 1" ABOVE FINISHED GRADE
 - B & B OR CONTAINERIZED (SEE SPECIFICATIONS FOR ROOT BALL REQUIREMENTS).
 - PREPARED PLANTING SOIL AS SPECIFIED.
 - ROOTBALLS GREATER THAN 24" DIAMETER SHALL BE PLACED ON MOUND OF UNDISTURBED SOIL TO PREVENT SETTLING ROOTBALLS SMALLER THAN 24" IN DIA. MAY SIT ON COMPACTED EARTH.
 - UNDISTURBED SUBSOIL.
- NOTES:**
- FINAL TREE STAKING DETAILS AND PLACEMENT TO BE APPROVED BY OWNER.
 - REMOVE BURLAP, WIRE AND STRAPS (ANYTHING THAT COULD GIRDLE TREE OR RESTRICT ROOT GROWTH) ON UPPER 1/3 OF ROOTBALL.
 - SEE LANDSCAPE NOTES FOR THE TYPE OF MULCH MATERIAL TO USE.
 - PRUNE TREE AS DIRECTED BY LANDSCAPE ARCHITECT.
 - BRANCHING HEIGHT TO A.A.N. STANDARDS.



2 CONIFEROUS TREE PLANTING DETAIL
L1.01 NOT TO SCALE



3 SHRUB/GROUNDCOVER PLANTING
L1.01 NOT TO SCALE

engineering surveying planning energy

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Client
HTG ARCHITECTS
9300 HENNEPIN TOWN ROAD
MINNEAPOLIS, MN 55347

Project
TCF BANK

Location
SHOREVIEW, MINNESOTA

Certification
I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly licensed LANDSCAPE ARCHITECT under the laws of the State of Minnesota.

Kevin Tegen
Registration No. 269XXX Date: XX.XX.XX

NOT FOR CONSTRUCTION

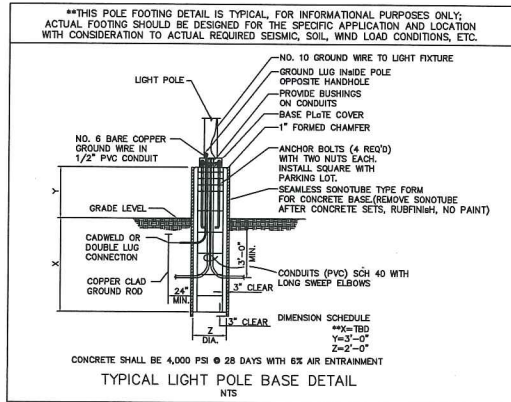
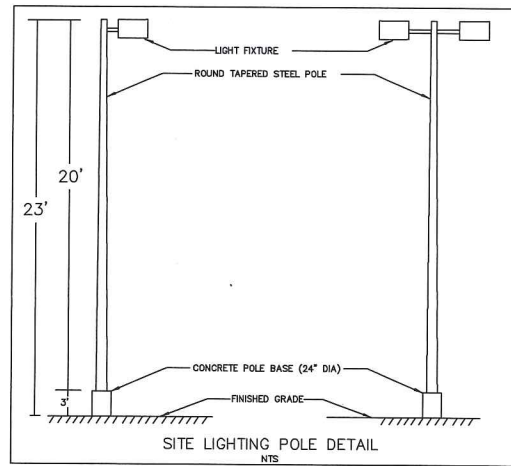
Summary
Designed: bcw Drawn: JN
Approved: MCA Book / Page:
Phase: PRELIM Initial Issue: 04/30/2012

Revision History
No. Date By Submittal / Revision
A 05/08/12 JN CITY COMMENTS

Sheet Title
LANDSCAPE PLAN

Sheet No. Revision
L1.01 A

Project No. HTG19272



TECHLIGHT

Project: TCF Bank, Type: A

Model: DCMB1-C-N-1-PS-X

ATLAS SERIES Medium Die Cast Area Light

Features & Specifications

Construction: The housing and door frame are constructed of die-cast aluminum. The flat tempered glass lens is sealed in place and a door gasket is provided for weather tight protection. ETL listed for wet locations.

Optical System: Made with a state of the art high performance reflectors that use high glass components to achieve precise photometric distributions. The fixture is a full cut-off light fixture and is Dark Sky Friendly in the horizontal position only.

Electrical System: Available in 120V line 400W standard metal holder, push-start metal holder, and high pressure sodium. Comes standard with a 40V pulse start mogul base socket. Each mogul base multi-tap or 400V ballast is pre-wired and installed for convenience.

Mounting: Standard mounting options include 2", 8" and 10.75" straight arms with integral bolt gaskets for square and round pole mounting on rheolux models. 2" adjustable slip fitter and 1/2" hole mount available for floodlight models. Additional mounting options are available.

Finish: A Super Durable Polyester powder coat finish is electrostatically applied in our state of the art paint facility. Standard colors available: Black, Bronze, US Green, White. Custom colors available upon request.

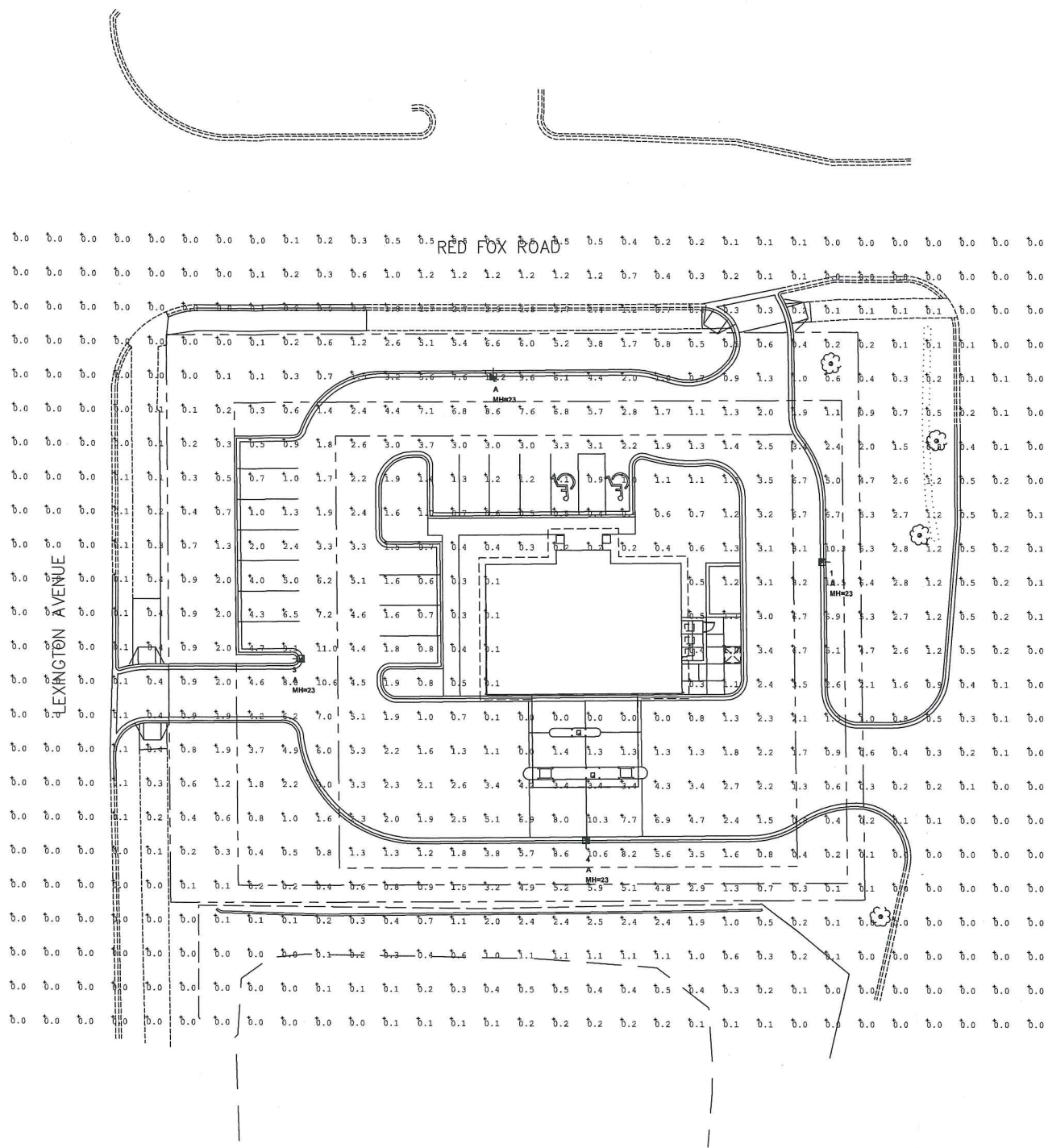
Net Shipping Weight: 22 lbs
EPA: 55 WATT

Passes and/or your Techlight Sales Representative for more information.
All dimensions subject to change without notice.

DCMB1	C	N	1	PS	X
SERIES	REFLECTOR	BALLAST/LAMP	VOLTAGE	PULSE-START	FINISH
DCMB1	A - Standard D - Cutoff H - Narrow J - Parabolic K - Medium L - Parabolic M - Type I N - Type II O - Type III P - Type IV Q - Type V	V - 150W PSMH D - 150W PPS L - 175W MH M - 250W MH E - 250W PPS T - 320W PSMH K - 320W PSMH N - 400W MH F - 400W PPS	1 - Multi-Tap 2 - 400V	PS - Pulse-Start	BK - Black BZ - Bronze GR - US Green WH - White SP - Special

Techlight Inc. 214.390.0691
2707 Bergasa Drive 800.226.9127
DALLAS, TX 75228
WWW.TECHLIGHTUSA.COM

LumNo	Label	Z
4	A	23
3	A	23
2	A	23
1	A	23



Symbol	Qty	Label	Arrangement	Lumens	LLF	Description
	4	A	SINGLE	42600	0.810	Techlight DCMB1-C-N-1-PS 400 Watt PSMH

Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
All at Grade Calc Pts	Illuminance	Fc	1.33	11.2	0.0	N.A.	N.A.
Parking Area	Illuminance	Fc	2.51	11.0	0.4	6.28	27.50

For Additional Lighting Information and Pricing Contact:

On-Site Lighting & Survey, LLC PH: 763-684-1548

Project #: 7120427
Contract #:
Issue Date: 04/27/2012
Scale: AS NOTED
Drawn/Checked: TDK/WRT
Sheet #: SL1 0

Based on the information provided, all dimensions and luminaire locations shown represent recommended positions. The engineer and/or architect must determine applicability of the layout to existing or future field conditions.

This lighting pattern represents illuminance levels calculated from laboratory data taken under controlled conditions utilizing current industry standard lamp ratings in accordance with Illuminating Engineering Society approved methods. Actual performance of any manufacturer's luminaire may vary due to variation in electrical voltage, tolerance in lamp and other variable field conditions.

On-Site Lighting & Survey, LLC

1111 Hwy 25 North, Suite # 201
Buffalo, MN, 55313
Ph 763-684-1548 Fax 763-682-9048

Revision: Date:

PHOTOMETRIC PLAN

TCF BANK SHOREVIEW, MINNESOTA

Project #: 7120427

Contract #:

Issue Date: 04/27/2012

Scale: AS NOTED

Drawn/Checked: TDK/WRT

Sheet #: SL1 0

PROJECT

**TCF NATIONAL BANK
 SHOREVIEW, MN**

NEW CONSTRUCTION

ISSUED SET	REVISIONS
DATE	NO.

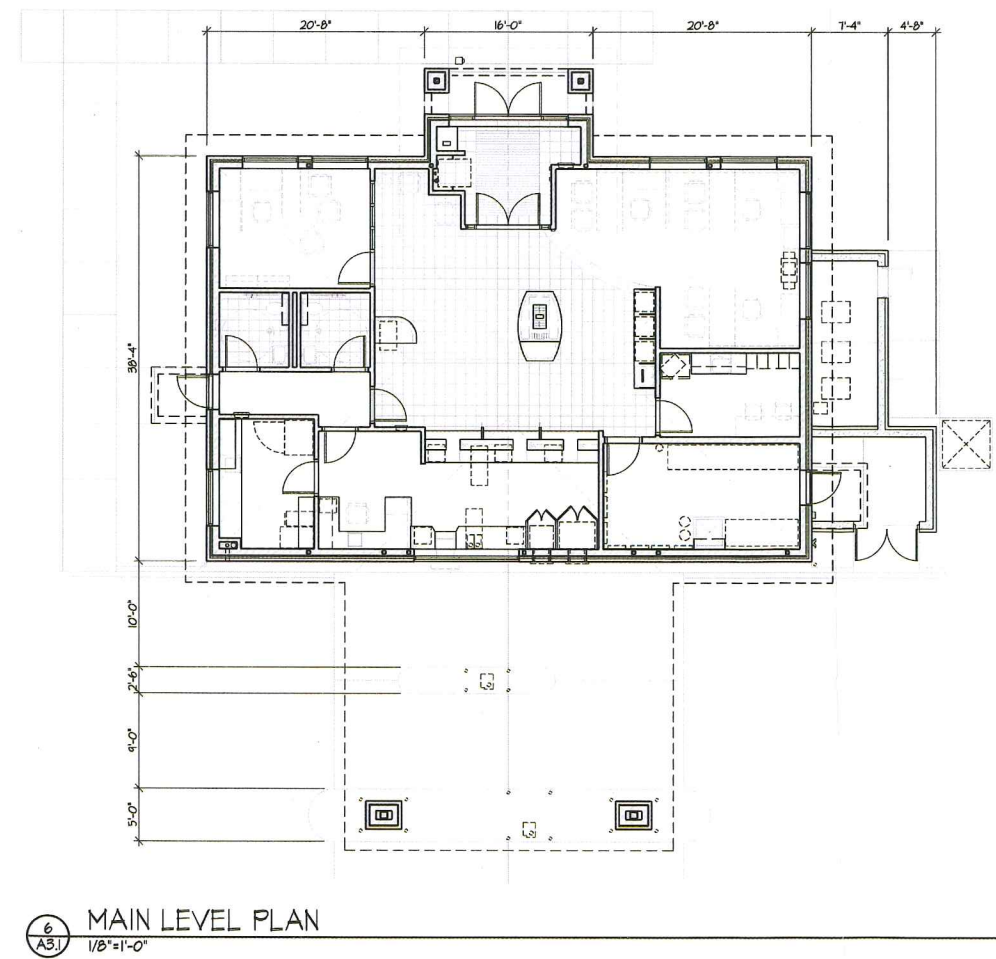
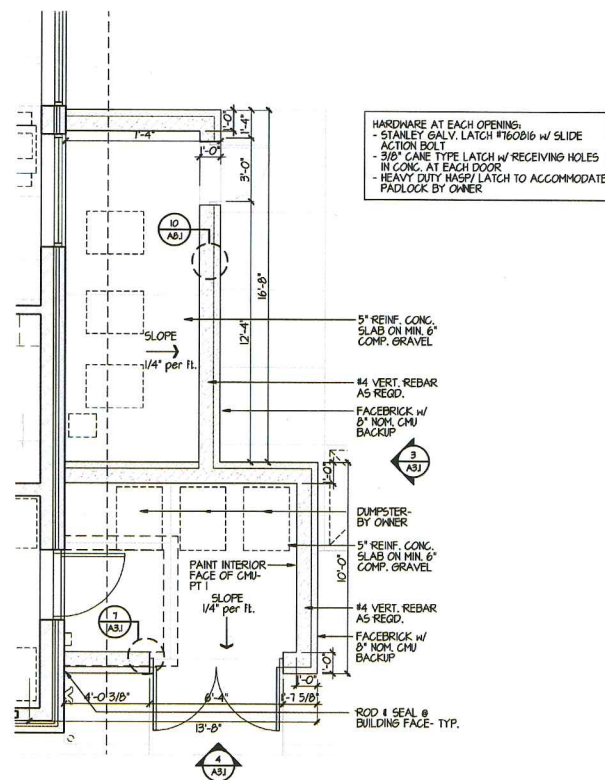
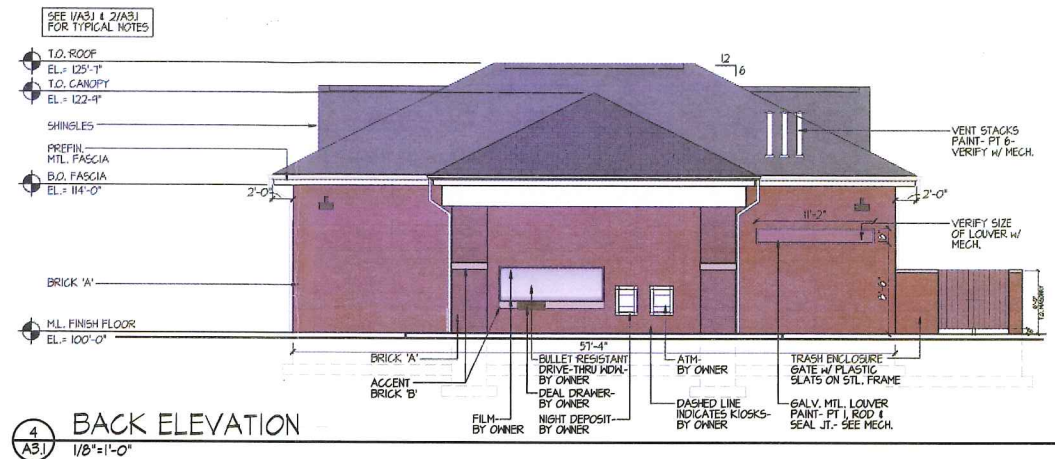
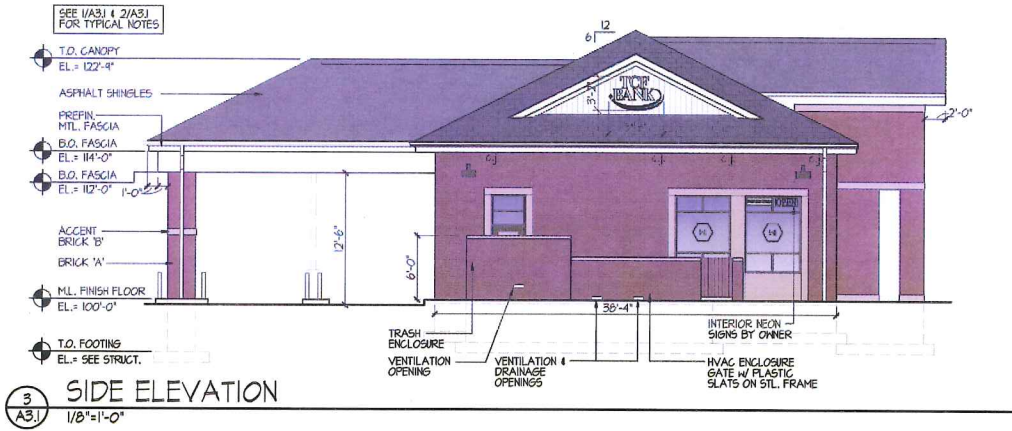
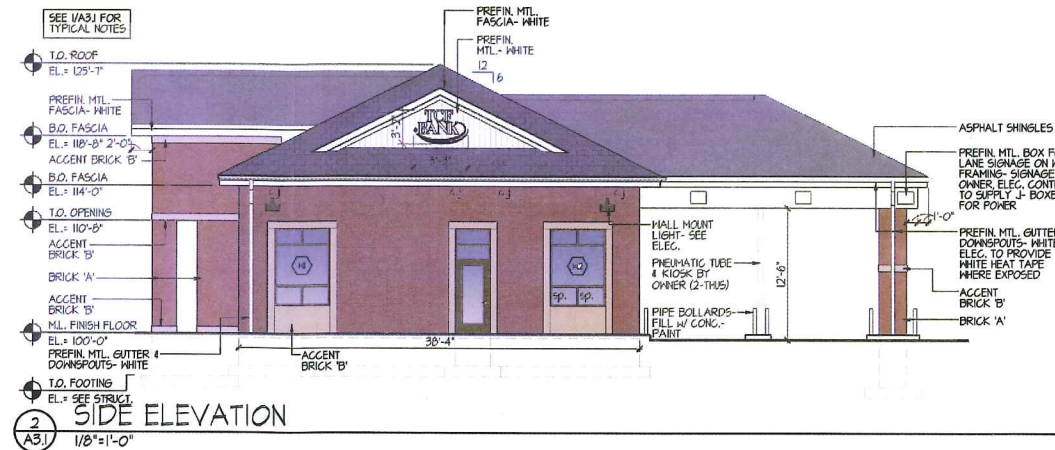
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION, AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA

MICHAEL F. KRAFT
 #23538 REG. NO. _____ DATE _____

EXTERIOR ELEVATIONS
 ENCLOSURE PLANS
 ENCLOSURE ELEVATIONS
 ENCLOSURE DETAILS

DRAWN BY: JDZ CHECKED BY: MFK

A3.1





Kathleen Nordine <knordine@shoreviewmn.gov>

Request for comment 3836 Lexington Avenue

1 message

David Carland <DCarland@stonehenge-usa.com>

Wed, May 16, 2012 at 10:56 AM

To: "KATHLEEN NORDINE (KNORDINE@SHOREVIEWMN.GOV)" <KNORDINE@shoreviewmn.gov>

Cc: Randy Rauwerdink <RRauwerdink@stonehenge-usa.com>

Kathleen, per the Request for comment that we received from the City we want to go on the record as supporting the redevelopment of the Sinclair property for a TCF Bank branch. As you know Stonehenge has a considerable investment in the shopping center we are developing to the east on Red Fox Road and believe that replacing the current filling station with a new well designed bank facility will enhance the market acceptance of our project and encourage additional quality development in the area.

We do not plan on attending the public hearing but please communicate our support to the Planning Commission.

Please note our new office location effective January 4, 2012

Dave Carland

Stonehenge USA

6007 Culligan Way

Minnetonka, MN 55345

O: 952.288.2202

C: 612.963.9107

dcarland@stonehenge-usa.com

www.stonehenge-usa.com

MOTION

MOVED BY COMMISSION MEMBER: _____

SECONDED BY COMMISSION MEMBER: _____

To recommend the City Council approve the PUD amendment permitting the redevelopment of the Sinclair property 3836 Lexington Avenue with a retail bank facility, subject to the following conditions:

Planned Unit Development – Development Stage

1. The property shall be developed in accordance with the plans submitted as part of this application. Revisions to the submitted plans may be permitted to respond to comments received from the City Engineer and Ramsey County representative. Said revisions shall be submitted as part of the Finals Stage PUD application.
2. The applicant shall continue to work with the City Engineer and Ramsey County representatives to resolve issues regarding road right-of-way, planned access points and future road improvements. These issues shall be addressed prior to the City's approval of the Final Stage PUD.
3. Prior to the issuance of a building permit, the applicant shall address the items stated in the memo from the Fire Marshal.
4. The applicant shall enter into a site development agreement and an erosion control agreement with the City prior to the issuance of a building permit for the project.
5. The existing public sewer and water utilities shall be relocated south of the proposed drive through facility and pavement area. The existing utility easement shall be vacated and a new easement established along the south property line.
6. A permit is required from Ramsey County for any work in the Lexington Avenue right-of-way.
7. The landscape plan shall be revised to provide a diversity of trees with a ratio of 30-20-10.
8. The applicant shall work with the City to develop a commercial gateway sign for the Red Fox Road retail area.
9. A Comprehensive Sign Plan is required prior to the installation of any signage on the property.
10. Authorize the Building Official to issue proper building permits after Final Stage approval of the PUD and execution of the development agreement.

This approval is based on the following findings of fact:

1. The proposed land use is consistent with the designated commercial land use in the Comprehensive Plan and PUD.
2. The redevelopment supports the City's commercial land use goals regarding the reinvestment in commercial areas and providing services which support resident needs and employment opportunities.
3. Redevelopment of this site provides the City with an opportunity to address roadway improvements and infrastructure needs in this commercial area.

VOTE:

AYES:

NAYS:

Regular Planning Commission Meeting

May 21, 2012

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